



Administrative Report

DISCUSSION ITEM

TO: HONORABLE CITY COUNCIL

FROM: James Vega, City Manager
Greg Grant, Public Works Director

DATE REPORT

PREPARED: February 1, 2021

MEETING DATE: February 10, 2021

SUBJECT: Maricopa Highway Demonstration Project and Active Transportation Program (ATP) Project Improvements Including Maricopa Highway and Ojai Avenue

Recommendation

That City Council:

- 1) Receive a report and presentation on the Maricopa Highway Demonstration Project and the Active Transportation Program (ATP) Project;
- 2) Provide direction to staff regarding modifications to the Demonstration Project; and
- 3) Provide direction to staff regarding modifications to the overall Active Transportation Program Project based on the Demonstration Project.

Executive Summary

At its December 11, 2018 Regular Meeting, City Council requested a demonstration of the Maricopa Highway lane reduction. The Maricopa Highway demonstration project is intended to demonstrate the Maricopa Highway portion of the overall Active Transportation Program (ATP) Project and related pedestrian and bike improvements. In April 2019, staff secured a \$430,000 grant to fund the 6-plus month demonstration from the Southern California Association of Governments (SCAG). This grant funding can only be used to support the ATP Project traffic calming and bike and pedestrian improvements with an emphasis on demonstrating the related lane reallocation.

On November 10, 2020, Council reviewed the project and provided a resolution of support for the demonstration project. The demonstration project was then installed the week of December 7, 2020.

This report provides an update of the Demonstration Project and related issues, and allows for discussion of desired modifications. As part of the Demonstration Project, Staff has begun collecting substantial data. This staff report is a summary of key points, further information is provided in the attachments.

Discussion

Active Transportation Program (ATP) Project Background

At its Regular Meeting on January 13, 2015, Council authorized a resolution of support for the Active Transportation Program (ATP) Project, soon after the City successfully secured the grant. At its Regular Meeting on December 13, 2016, Council reviewed the concept design. At its Regular Meeting on January 22, 2019, Council affirmed the design and priorities, following outreach efforts.

At its Regular Meeting on June 11, 2019, Council authorized a contract with Alta Planning and Design to prepare design documents necessary to secure a Caltrans permit for construction of the ATP Project in 2021-22.

At its Regular Meeting on February 11, 2020, Council reviewed the “35 percent” completeness level design and cost estimate, and at its Regular Meeting on June 9, 2020, Council reviewed the “60 percent” completeness level design and cost estimate. As requested by Council, the City obtained grant funding for a demonstration of the Maricopa Highway lane reduction, which is now underway. Further details of the demonstration project are provided below.

The demonstration project was initially intended to be in place for 6 months. As a result of the pandemic, staff requested and received an extension of the permanent project funding to assure time for a potentially longer demonstration, if Nordhoff High School’s schedule does not return to normal until fall of 2021. This extension also included any time necessary to incorporate Council-directed changes to the design.

At this time, planned further design of the permanent ATP project design is being delayed to allow for Council assessment of the demonstration project and potential incorporation of any lessons learned into the final design. This also allows an opportunity for staff to pursue additional grant funding.

As discussed previously, the demonstration project and permanent design effort does not commit the City to construction of the project. That would occur at the sole discretion of the Council, when the City is ready to award a construction contract, in 2022 or later (see schedule below).

This staff report allows Council to review desired options, including allowing Council to consider potential modifications which would be possible through grant funding, from minor to major, including the following:

- 1) Continue as is with the current demonstration project design, with minor modifications to the design as described in Attachment C.
- 2) Continue the demonstration project with more substantial modifications such as those described in the “permanent” section in Attachment C.
- 3) Revise the design to the pre-demonstration four lane configuration, with separated bike lanes along the curb. This approach is possible but would not allow sufficient space to provide parking, turn lanes, or a school drop off zone.

Further detail regarding these options is provided in section “D” of this report.

Demonstration Project Background:

A several-month-long demonstration of the Maricopa Highway lane reallocation was the recommended approach to “test drive” the lane reallocation and related pedestrian and bike improvements. A demonstration of the lane reallocation concept with parking protected bike lanes and curb extensions has been permitted through a SCAG-administered grant for \$430,000.

Renderings with photos of the site before the project, during the demonstration, and after planned permanent improvements project are included as Attachment A. The demonstration plans are included as attachment H. A higher quality version, is also available on the City’s website. It can be reached by visiting <https://ojaicity.org/goojai/>. Members of the public can also request copies by contacting the City’s Public Works Department at 805-646-5581, extension 200.

The demonstration uses temporary features such as delineators, paint, and planters to simulate the proposed final design. All demonstration elements, including planters and curbing, are temporary and can be moved or removed.

The demonstration will continue for at least six months. At that time, if Nordhoff High School has not resumed its full, in-person session and with a reasonable overlap to enable testing of peak start/finish traffic at the school, the demonstration will continue to assure an overlap for a few months while school is in full session. This contingency may require the demonstration to continue into the fall of 2021. Any lessons learned or improvements desired in the demonstration will be periodically recapped for Council consideration for incorporation into the final design. This report is the first of those reviews.

Demonstration Project – Status:

The project was installed the week of December 7th 2020. It started with lane number two (right lane) closure in both directions, and continued with street-marking removal and placement, planter and delineator installation, and planter soil and plant placement. It also included hundreds of temporary orange delineators to cordon off the area to allow work to proceed safely in the second lane and shoulder area.

Approximately 50 volunteers helped with the installation. The temporary traffic control was removed on December 11th, 2020, and the demonstration began. Traffic patterns, public feedback, incidents, and lessons learned follow:

A. Traffic Patterns: Two months into the Demonstration Project, the following traffic is being monitored with any trends noted. Note that the Covid-19 pandemic likely affects these results; a Stay-at-Home order began the same week as the Demonstration Project was implemented, and the order ended on January 25th, 2021. Longer-term monitoring and use of the demonstration will better define potential changes to traffic patterns.

- *Vehicle speeds and volumes*: are being monitored between Pirie and Church roads and between Church Road and the adjacent meadows. Additional volume monitoring is being done at the “Y”, Vallerio Avenue, Pirie Road, and Church Road intersections. The average speed (50th percentile) prior to the Demonstration was 37 mph (four weeks in October/November 2020), after it was 35 mph (three weeks in January 2021 to date, monitoring will continue throughout project), per Attachment I. The number of vehicles exceeding 40 mph before the Demonstration was 28% of traffic (1,055

vehicles per day average), after the Demonstration it was 13% (440 vehicles per day average), a 58% decrease in speeding – this was expected with the removal of the passing lane. The average daily traffic volume prior to the Demonstration was 3,700 vehicles per day, after the Demonstration it was 3,392 vehicles per day, a 8% decrease – this is likely a result of the stay at home order and the Holidays, as the cut-thru monitoring did not indicate an increase in traffic through adjacent streets of Cuyama and Hermosa Roads (see discussion below). These numbers may normalize now that the Stay-at-Home order has ended. Staff will continue to monitor and report back.

- *Bike and Pedestrian Activity:* Numina traffic monitoring devices were installed at four intersections (at the “Y”, Vallerio Ave, Pirie and Church Rd) prior to the project and will continue monitoring throughout the demonstration. The initial data indicates the following trends.
 - Pedestrian activity increased on average by 28% after the Demonstration, with some locations increasing by as much as 86%. Counts vary from 128 to 223 pedestrians per day after the demonstration implementation.
 - Bicycle activity increased on average by 19% after the Demonstration, with some locations registering increases as high as 48%. Counts vary from 23 to 173 bicyclist per day after the demonstration implementation.
- *Cut-thru traffic:* Changes in traffic volumes north and south of the Demonstration are being monitored on Cuyama and Hermosa Roads to see if vehicle traffic is diverting onto these streets to avoid the Demonstration project. On Hermosa Road, the average daily traffic volume prior to the Demonstration was 760 vehicles per day, after it was 770 vehicles per day, a 1% increase. On Cuyama Road the average daily traffic volume prior to the Demonstration was 1,479 vehicles per day, after it was 1,235 vehicles per day, a 17% decrease. These figures indicate cut-through from the Demonstration Project is not occurring at this time. This is initial data, potential cut-through traffic will continue to be monitored throughout the Demonstration project for potential trends.

B. *Emergency Response:* The Ventura County Fire Department drove two full-size fire engine vehicles in the bike lane as a test, to see if the lanes could be used for emergency detour. The demonstration was successful although there was a pinch point in the demonstration project on the southbound side at the right turn into Nordhoff High School, which slowed the fire engines. This pinch point is being removed from the permanent design. Additionally, the Fire Department provided a written statement to City staff that the proposed design does not negatively impact their emergency response. Additionally, we received positive feedback from the Police Department and Lifeline ambulance service and have not received any concerns from these emergency services providers.

C. *Public Comment/Feedback:* The two major goals of the Demonstration Project were to identify any needed design modifications prior to implementation of the permanent project, and also to receive feedback from the Community regarding the proposed changes. As discussed in this report, the City has been able to identify and make design modifications, and the City has received substantial public comment and feedback.

As of January 27th, there have been 501 surveys completed on the City’s ATP Demonstration website. The survey questions with a summary of responses for each

question are included as Attachment E. Most of the responses were received in the days immediately following implementation of the demonstration, although the City has continued to receive responses. Staff has highlighted two key questions and responses, with more information available in Attachment E:

- Question 20. Do you support the City's ATP Project for this stretch of Maricopa Highway?
 - Response: 41% yes, 59% no.
- Question 22. Do you support permanent bicycle and pedestrian improvements to Maricopa Highway, and why?
 - Response: 56% yes, 44% no.

The City has also received a number of public comments via email and social media, both in support and in opposition to the project. Written public comments submitted for City Council meetings since the implementation of the Demonstration Project are available for review at: <https://ojaicity.org/public-comments/>. A brief summary of common issues and concerns are included as Attachment F. The City has also prepared and distributed the Frequently Asked Questions (FAQs), to address the most common concerns and questions, included as Attachment B, and posted on the City's ATP Demonstration webpage.

D. *Potential Modifications to the Demonstration and/or Permanent Project:* Staff closely monitors the demonstration project. While it takes time to fully demonstrate the project and how driver, pedestrian, and bicyclist habits will change, staff has implemented or is considering the modifications shown in Attachment C, with some early considerations for changes to the permanent project as a result of the demonstration.

City Council may consider the following options for modifications and provide direction as to any desired changes. Here is a summary of the primary options, with further descriptions in Attachment C.

- 1) Continue as is with the current demonstration project design, with minor modifications to the design as described in Attachment C. This approach has already been designed and reviewed with Caltrans and is the default approach and budget. Staff expects this approach to provide the greatest benefits to bicycle and pedestrian safety and traffic calming.
- 2) Continue the demonstration project with more substantial modifications such as those described in the "permanent" section in Attachment C. This approach would require review and approval with Caltrans (estimated at 1 month review time), depending on the extent of the changes.
- 3) Revise the design to the pre-demonstration four lane configuration, with separated bike lanes along the curb. This approach is possible but would not allow sufficient space to provide permanent parking, turn lanes, or a school drop off zone. This approach would require consultant redesign effort and review and approval with Caltrans.

Any substantial cost to modify the demonstration project could be paid for by grant monies, however it would reduce funds available for construction of the permanent project.

When considering options, it is important to understand the benefits and disadvantages of a lane reduction:

Expected Benefits:

- Pedestrians: For pedestrians, crossing 2 lanes is safer than crossing 4. The pedestrian need only focus on looking for oncoming cars in one lane in each direction instead of two, one of which may be a speeding car. The time the pedestrian is exposed to traffic is roughly twice as long.
- Bikes: The reallocated space provides more space for a separated bike lane.
- Cars/Vehicles: It is simpler to judge one lane of traffic in each direction when turning left across traffic and exiting streets or driveways.
- Calmer traffic: There is no longer a passing lane, one lane better limits speeding as one car going the speed limit controls the speed of all cars. And the more confined feel of a single lane tends to calm traffic.

Expected Disadvantages:

- Inability to pass and the confined feel of one lane bothers some drivers.
- A car turning into a street or driveway without a turn lane may slow in the primary lane, which may slow the one lane of traffic. Note there is left and right turn lanes provided where heavier traffic is anticipated, and there is a shoulder in most areas that can be used as a turning area as well.
- A single lane may space traffic more, making left turns from streets or driveways on the highway more difficult. Numina traffic devices are monitoring this concern (dwell time), preliminary data indicate no change, but staff will report back.

E. Incidents: There has been a bicycle accident reported and many planters vandalized or accidentally damaged. Staff met with the parties involved in the bicycle accident to assure a full understanding. See Attachment G for a summary.

F. Costs: Staff provided a breakdown of the ATP Permanent project cost at the June 2020 Council meeting. The design consultant has further refined the estimate, which is included as Attachment D. A summary of the Pedestrian, Bike and other improvement costs are as follows:

	<u>Pedestrian</u>	<u>Bike</u>	<u>Utilities</u>	<u>Subtotal</u>
	<u>Improvements</u>	<u>Improvements</u>		
Maricopa	\$478,193	\$410,667	\$18,600	\$1,216,397
Ojai Ave	\$1,855,608	\$104,571	\$137,150	\$2,478,564
Subtotal	\$2,333,801	\$515,238	\$80,100	\$3,694,961
Total				\$3,694,961

Schedule:

The following is the overall schedule for the ATP Project with flexibility for unknown Covid-19 impacts:

- Demonstration Project: December 2020 – at least May 2021. Likely to be extended to assure High School is back in full session with related peak start/finish of school traffic, likely into the fall of 2021.
- Incorporation of Council direction to revise Permanent Design with Caltrans: Within months of the end of the demonstration project, as early as the fall of 2021, as late as April 2022.
- Award of Construction Contract: As early as April 2022, as late as October 2022.
- Construction: Starting as early as summer/fall 2022, or as late as winter/spring 2023 (approximate 6 to 12-month construction period).

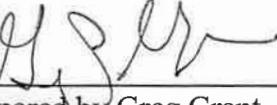
Fiscal Impact

The Maricopa Highway Demonstration Project is funded by a \$430,000 SCAG grant with support of City Public Works staff and volunteers.

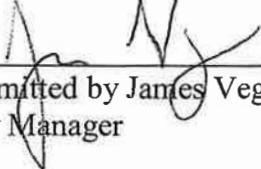
The permanent ATP Project for Maricopa Highway and Ojai Avenue, has received a total of \$2.8 million in grant funding for the ATP project. Of that, approximately \$2.3 million is available for construction.

- \$2,300,000 for construction.
- \$373,000 for design work.
- \$105,000 for right-of-way easements/acquisitions.
- Although no City match was required, the City spent approximately \$130,000 from 2015 through 2019 from Fund 31 on preliminary design and outreach efforts.

With the estimated total cost of \$3,694,961, approximately \$1.4 million in additional funding is needed to construct the current design. Staff has applied for additional grant funding to support the ATP Project costs as authorized by Council at its Special Meeting on June 16, 2020. If grant funding is not received by April, staff will return to Council with recommendations to reduce costs.



Prepared by Greg Grant,
Public Works Director



Submitted by James Vega,
City Manager

Attachments:

- A – Renderings of Pre-Project vs. Demonstration Project vs. Permanent Project
- B – Frequently Asked Questions (FAQs)
- C – Potential Modifications to the Demonstration and/or Permanent Project
- D – ATP Project Cost Estimate
- E – Survey Summary
- F – Public Comment Summary, all comments available at: <https://ojaicity.org/public-comments/>.
- G – Incidents
- H – ATP Demonstration Project Plans
- I - Traffic - Volume and Speed Data Summary

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Attachment A

Renderings of Pre-Project

vs. Demonstration Project

vs. Permanent Project

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Permanent Project (renderings):

Meadows Preserve (just north, southbound lane):



High School Trolley Stop (just north, southbound lane):

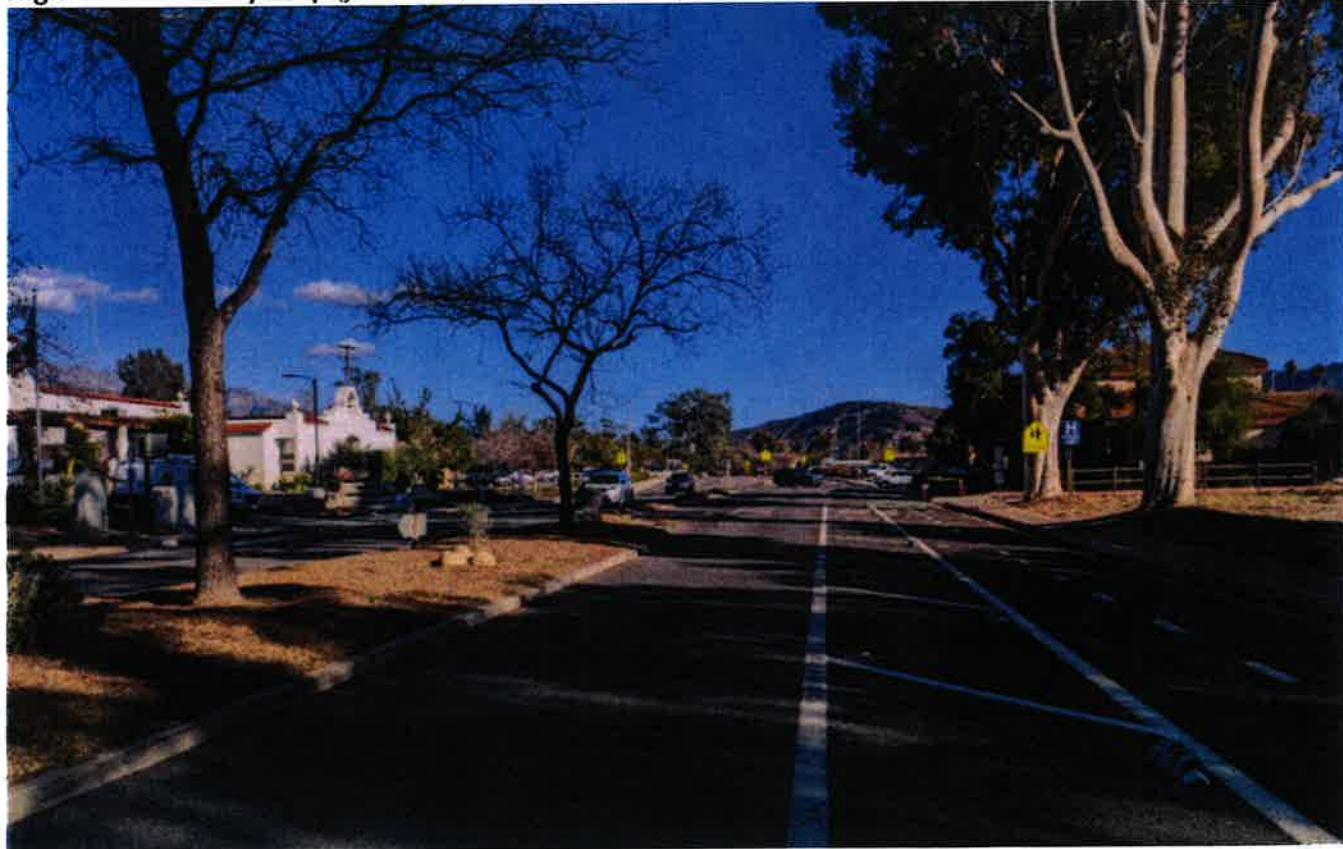


During Demo Project (actual photos):

Meadows Preserve (just north, southbound lane):



High School Trolley Stop (just north, southbound lane):



Before Demo Project (actual photos):

Meadows Preserve (just north, southbound lane):



High School Trolley Stop (just north, southbound lane):



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Attachment B

Frequently Asked Questions

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Frequently Asked Questions (FAQ)

CITY OF OJAI

Updated January 14, 2021

ACTIVE TRANSPORTATION PROJECT (ATP)

MARICOPA HIGHWAY DEMONSTRATION

The FAQs are grouped in the following order:

1. General Project Information
2. Pedestrian & Bike Safety
3. Project Review & Support
4. Design Information & Changes
5. Background Studies
6. Traffic Monitoring
7. Emergency Evacuations & Response
8. Public Input/Feedback/Comments

The Project is also known as the "Go Ojai Demonstration Project", with grant funding from SCAG (Southern California Association of Governments) who administered the Consultant, Street Plans.



ojaicity.org/goojai

Instagram: @go_ojai

STREETPLANS
New York



1. GENERAL PROJECT INFORMATION:

- **What is a demonstration project?**

A demonstration project is a temporary installation of infrastructure in anticipation of a longer term, permanent project. A demonstration project allows for modifications and feedback before a permanent project.

- **What are the goals of the demonstration project?**

The primary goal of this project is to demonstrate the Council supported concept design for the permanent ATP Project for Maricopa Highway between Ojai Avenue (at the Y) and Cuyama Road. The plan provides safety and beautification improvements including reallocating the second vehicle lane, improved pedestrian crossings, and on-street protected bike lanes. The permanent plan includes adding concrete curbed landscape pockets, and sidewalks and trees where missing, but these features will not be included as a part of the demonstration project.

- **When was the demonstration installed?**

The demonstration project was installed from December 7 to 12th, 2020. The project will be in place for a minimum six-month period, and extended as necessary to assure overlap with school back in full resumption for months to properly evaluate the demonstration with normal school and business traffic. The permanent project is planned for construction approximately a year after the demonstration ends, so roughly in the Summer to Fall of 2022. COVID-19 delayed the implementation of the demonstration project, which will delay the construction of the permanent project.

- **Will the demonstration project continue until Nordhoff High School is back in full in-person session?**

Yes! The demonstration will be extended to assure an extended demonstration occurs with the High School in full session. We hope that occurs soon, this Winter or Spring, but are ready to extend the demonstration until the Fall if necessary.

- **Where can I find information about the demonstration and permanent project?**

- The ATP demonstration project has a webpage on the City's website: ojaicity.org/goojai
- The permanent ATP Project has a webpage on the City's website: ojaicity.org/atp-grant-page/
- The project has an Instagram social media account: [@go_ojai](https://www.instagram.com/@go_ojai)
- The City is posting updates on its Facebook page: www.facebook.com/cityofojai/

- **What types of materials are used?**

Demonstration projects use temporary materials such as paint and installation methods that do not require heavy construction as opposed to the more permanent materials and installation methods (e.g. concrete curbs, sidewalk, and medians, moving curbs, etc.). This demonstration project uses materials that are approved by Caltrans including traffic marking paint, temporary low-profile delineators, and temporary surface-mounted planters. These planters will NOT be used in the permanent project, as the permanent project includes trees and landscaping in place of planters.

- **How does the lane take into consideration trolley stops?**

There are breaks in the bike lanes at every trolley stop to allow trolleys to pull in curbside for ADA access.

- **Where is the funding coming from for the demonstration project?**

The City received a \$430,000 grant from the Southern California Association of Governments (SCAG). It can only be utilized to demonstrate safety improvements that enhance car, bike, and pedestrian safety as outlined in the grant application.

2. PEDESTRIAN & BIKE SAFETY:

- **How does this benefit Cars, Pedestrians, and Bikes?**

The permanent project includes traffic calming (primarily the lane reduction), bike improvements (the protected bike lane), and pedestrian safety improvements (primarily the lane reduction – crossing 2 traffic lanes is much safer than crossing 4; and curb extensions – highlighting and protecting pedestrians off the typical curb alignment out toward the travel lane). These improvements are intended to increase the safety of all modes of travel, including cars. The Demonstration project is able to demonstrate the concept of a lane reduction and bike lane improvements, although it is recognized in a temporary/ demonstration form they are not as appealing as the permanent improvements with concrete curbs, landscaping, etc.

- **What's the benefit of the 2-way bike lane?**

The lane was intended to enhance safety for school kids coming from housing behind Vons to the High School – to avoid making them cross Maricopa Hwy multiple times.

The safety benefits of a 2-way bike lane may outweigh the negatives, although each driveway or street crossing has some risk. Signs at driveways warning/reminding bikes and cars to be extra cautious such as the following will help:



- **Why are there still bicycles riding on the sidewalk?**

It will take some time for habits to be broken. Some bicyclists are still using the sidewalk to travel north on the southbound side (school side), as the bike lane does not allow travel in this direction for most of the way – see Lessons Learned section below addressing this. Some bicyclists are still riding the sidewalk going southbound, as they developed the habit when they felt unsafe being on the highway shoulder.

PROJECT REVIEW & SUPPORT:

- **Who has reviewed the project?**

The conceptual permanent and demonstration designs were developed by the City in consultation with several traffic engineering firms, reviewed and supported by the Ojai City Council, Caltrans, the Ventura County Sheriff/City of Ojai Police Department, the Ventura County Fire Department, the Ojai Unified School District, the Ojai Valley Community Hospital, and LifeLine Ambulance. Utility agencies including the Ojai Valley Sanitary District and the Casitas Municipal Water District have provided review of the plans. The demonstration project design has been reviewed by members of the demonstration project Technical Advisory Committee and comments have been incorporated. The final plans under review by Caltrans are being designed by Alta Planning and Design, a transportation planning, design, and implementation consulting firm.

- **What entities have expressed support for the project?**

- Ojai Unified School District
- Ventura County Sheriff/City of Ojai Police Department
- Ventura County Fire Department
- Ventura County Health Care Agency
- Ventura County Transportation Commission
- Ventura County Board of Supervisor Bennett's Office
- Ojai Valley Bicycle Coalition

3. DESIGN INFORMATION & CHANGES:

- **Does the project allow for iterating adjustments from experience gained from initial implementation design?**

Yes! The benefit of doing a demonstration is that adjustments can be made to the permanent project. The City and ATP project team will evaluate input from the demonstration project (both public comment and technical function) and consider if any adjustments need to be made to the final design for the permanent project.

- **Can you describe what has been implemented at the intersections near Nordhoff High School?**

There is a dedicated right turn lane into the high school parking lot for the south/eastbound lane (driving toward Y), which merges alongside vehicles turning into the parking lot from the north/westbound left turn pocket using dashed guide lines across the intersection. There is a dedicated pick-up/drop-off zone in front of the high school

between Church and Pirie Roads. Pedestrian crossing distances and the related exposure to a vehicle accident will be reduced for school students at Church Road by eliminating one lane in each direction and with curb extensions. U-turns at the Meadows Preserve to head southbound, for right turns into the school, are allowed to minimize use of the left turn.

- **What improvements have been made since installation of the demonstration project?**

The City and consultants are monitoring the demonstration project and public feedback to consider immediate and longer term changes. Note it will take time to cycle through receiving public feedback, adjusting the project, and for drivers to get used to the new layout.

The following is the primary constructive feedback and related changes:

A. The bike lane paint is too dark of a green.

The paint is being changed to the brighter lime green.

B. Drivers exiting driveways at the 2-way bike lane do not expect bikes coming from the right (the contra-flow direction).

Signage is being added at the driveways to look both ways for bikes, and the bike lane has signage cautioning of cars exiting driveways.

C. The planters are too close to park between.

The existing planters are spaced with a 40 feet gap between them, which is two standard parallel parking spaces. This should provide sufficient space to pull directly into the parking stall without a need to backup. Similarly, a car should be able to pull directly out. The City is considering removing every other planter to provide even more space, this will provide roughly 85 feet between planters, or 4 standard parking stalls.

D. Opening a car door in the parking lane is too close to the main travel lane.

The Demonstration project provides a 12-foot wide travel/drive lane plus an 8-foot-wide parking, which are Caltrans standards. This occurs throughout town, including Ojai Avenue through downtown. However, the concrete median on Maricopa Hwy does restrict the ability for cars to shift further left as provided downtown where there is an open median. We are considering options to widen the parking lane or create a buffer between the parking lane and travel lane to improve this situation.

Further, City staff has implemented or is considering the following changes on the project:

Immediate Modifications (already made or in process):

- Placed reflectors above planters on lathe to assure cars can see the planter as they backup or pullout.
- Adjust delineators (aka - "armadillos") to better accommodate U-turns.
- Adjust delineators (aka - "armadillos") to better accommodate right turns into side streets and driveways.

- Per above, add temporary signage warning bicyclists on the 2-way bike lane to look for cars exiting the driveways, and for cars to look for bicyclists, and where 2-way bike lane ends.
- Per above, brighter green paint for the bike lane conflict areas.
- Per above, remove every other planter to allow more space to maneuver while parking along the Meadows Preserve.

4. BACKGROUND STUDIES:

• What background analyses have been done for this project?

The demonstration project is adhering to the Council supported concept design for Maricopa Highway. The background studies that have been conducted for the project include: Pedestrian and Bike Safety Improvements Traffic Impact Study (2018), Supplemental Traffic Evaluation Report of the Functionality of Maricopa Highway (SR 33) during Emergencies, Caltrans Speed Zone Survey (2019) and 35% Design Plans for the Final ATP project (currently under Caltrans Review, 90, and 100% plans to follow). For the demonstration project, the project team produced an existing conditions summary, including, available on the ATP demonstration project webpage on the City's website at ojaicity.org/goojai.

- Corridor land use maps
- Relevant existing policies, ordinances, and studies including:
- 2012 Complete Streets Policy with Resolution supporting active transportation including pedestrian and bike improvements.
- 2015 Council Action supporting the ATP Grant application.
- 2017 Complete Streets Master Plan Adoption (include lane concepts for Maricopa Highway)
- 2018 ATP Project Pedestrian and Bike Safety Improvements Traffic Impact Study
- 2019 ATP Project Supplemental Traffic Evaluation Report of the Functionality of Maricopa Highway (SR 33) during Emergencies
- 2019 Caltrans Speed Zone study
- Existing pedestrian and bicycle facilities adjacent to/on the project corridor
- Mode share and demographics of walking and biking in the census tracts
- Multimodal traffic volumes and traffic operations
- Collisions along the project corridor
- Traffic speeds along the corridor

- Connectivity: How safely and easily can all street users use the project corridor to access jobs, school, open spaces, and other destinations?

5. TRAFFIC MONITORING:

- **What type of data is being collected for this project?**

Vehicle, bike, and pedestrian counts will be collected during the evaluation period, as well as vehicle speeds. The project team has deployed data collection devices along the corridor that will monitor vehicle volumes and turning movements in real-time during the six-month demonstration. This will allow the project team to analyze potential traffic backups, spillover traffic onto adjacent streets, and truck turning movements. The project team will also be collecting public feedback, and speaking with the school and businesses along the corridor before and during the demonstration.

- **What Traffic Monitoring is occurring?**

A. For traffic speeds:

- Pre-Project: 3 weeks prior to the Demo start, speeds were monitored in 2 locations, in addition to Caltrans data going back decades.
- During Project: speeds are being monitored at the same locations throughout the remainder of the project.

B. Counts and turning movements:

Traffic counts/volumes and turning movements for cars, trucks, bicycles, and pedestrians are being monitored at the Y, ValleRio Ave, Pirie Rd, and Church Rd. This monitoring began on September 27 and will continue for at least 7 months.

C. Cut-through traffic:

Potential cut-through traffic on Cuyama Rd and Hermosa Rd are being monitored with a survey before and periodically after. Additionally, the counts with turning movement data can be used to monitor cut-through traffic.

- **What if cars avoid using Maricopa Highway, cutting-through on Cuyama Rd or Hermosa Dr?**

Traffic volumes for cars, trucks, bicycles, and pedestrians is being monitored at the Y, ValleRio Ave, Pirie Rd, and Church Rd. Potential cut-through traffic on Cuyama Rd and Hermosa Rd are being monitored for potential increase. While significant increases are not expected, there are measures that can be implemented to discourage cut through traffic.

- **How will this project effect the speeds and speeding on Maricopa Highway?**

Two lanes provide a lane to pass and promotes speeding. A single lane better regulates speeds as one car traveling the speed limit will govern the following cars. This is expected to reduce the amount of vehicles traveling above the 35 mph speed limit for this area.

- **With only one lane, cars slowing to turn into a street or driveway are slowing all cars in the only remaining lane.**

Where substantial traffic surges are expected, a turn lane has been provided, such as at the High School's Church Rd entry southbound. Additionally, a left turn lane into the High School at Pirie Rd and at the staff parking lot are being considered. The rest of the driveways have minimal traffic and do not appear to need a turn lane. As part of the demonstration and feedback, there is consideration for removing the planters in more areas and/or providing more turnout lanes.

6. EMERGENCY EVACUATIONS & RESPONSE:

- **Have emergency evacuations been evaluated as part of the project?**

Yes, an analysis of potential impacts of the project on emergency evacuations was conducted. The study was reviewed and supported by the Police/Sheriff and Fire Departments, Caltrans, and City Council. As a result of the study, some adjustments were made to the permanent project design, primarily lengthening the parking between landscaped areas to assure larger emergency vehicles can park or traverse into the bike lane in case an emergency detour lane is needed. The study found no impact on the ability to evacuate during an emergency, as a reconstructed 2-lane segment will have the same capacity as each of the six 2-lane roadways that provide ingress/egress for the valley, so a reconstructed 2-lane segment will not create a bottleneck for vehicles accessing the other 2-lane roads in the valley. The study found no significant impact on emergency evacuation procedures. The design of the bikeway allows for its use as a detour or second vehicle lane during emergencies and lane closures.

- **Will first responders be able to use the bike lane and buffer as an emergency access lane?**

Yes. The design of the bike lane and buffer allows use of the bike lane as a detour lane for emergency and regular vehicles in the event the one remaining travel lane needs to be closed for any reason. Also, trucks and emergency vehicles will be able to clear the low-profile delineators that will line the bike lane buffer.

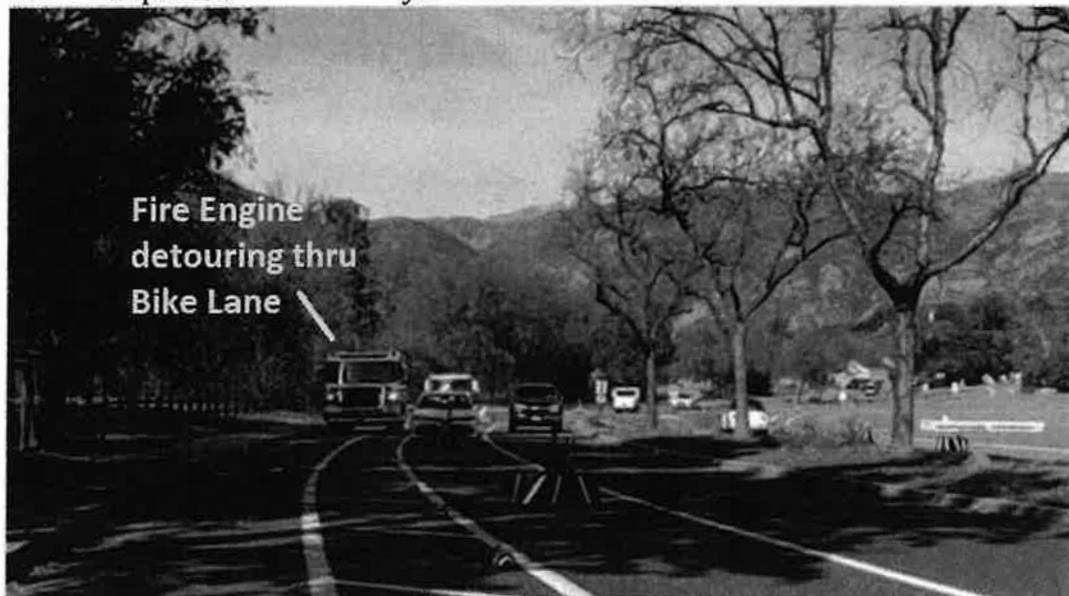
- **Where do cars pull over if an emergency vehicle is behind them?**

The gaps between the planters in the parking lanes is 40 feet – ample room for most vehicles to pull over directly without a backup maneuver. Historically, the greatest demand

for parking has been along the Meadows Preserve, and occasionally on the northbound side for school events (football games, 4th of July, graduation, etc.). Parking has not previously been legal in any of these areas, although not enforced. If an emergency vehicle needs to get through, this parking lane can be used to pull over, or the gaps between parking areas such as the school drop off zone and other non-parking zones can be used. There is consideration to increase this gap from 40 to 80 foot.

- **Where do emergency vehicles go if the one remaining lane is blocked?**

The bike path has been specifically kept wide enough to handle any emergency traffic. Similarly, the bike lane can be used for traffic if a utility is using the one lane, or if a tow truck had to access an accident. A demonstration using the bike lane as a detour with fire trucks was completed successfully.



7. PUBLIC INPUT/FEEDBACK/COMMENTS:

- **Will the demonstration allow for enough time for the public to provide comments before the permanent project is permitted?**

Yes, the public can comment on the project at any time while the demonstration project is occurring. Signs have been posted along the route with a link to an online survey to submit feedback. At the end of the demonstration the City Council will consider any lessons learned for incorporation into the final design before construction occurs. The survey is on the City's Go Ojai Demonstration webpage (right side button), or directly at:
<https://www.surveymonkey.com/r/FS6HXHM>

- **How is the team collecting public input?**

There was a **Community Advisory Committee (CAC)** that was open to the public, which met every other week prior to the project installation, over 15 meetings, **and will meet for**

a “debrief” meeting after the installation (TBD). The CAC is a hands-on working group that is involved in the implementation and evaluation of the project. All past CAC meeting recordings are on the project webpage. Please register on the city webpage for the debrief meeting. Additionally, several public Question/Answer sessions were held before the project installation.

Surveys were distributed both before and during the demonstration to gauge the public response to the project. You can now take the post-implementation survey on the City's Go Ojai Demonstration webpage (right side), or directly at: <https://www.surveymonkey.com/r/FS6HXHM>

- **How do I provide feedback?**

You can now take the post-implementation survey on the City's Go Ojai Demonstration webpage (right side button), or directly at: <https://www.surveymonkey.com/r/FS6HXHM>

- **Will the City consider the feedback I provide?**

The City and consultants are closely monitoring the demonstration project and public feedback to consider immediate and longer term changes. The City Council will periodically review the project including public feedback during the demonstration project for any final decisions on the permanent project. As noted above, feedback is already being incorporated.

Attachment C

Potential Modifications to the Demonstration Project

and/or Permanent Project

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Potential Modifications to the Demonstration and/or Permanent Project:

Immediate Modifications That Have Been Implemented As A Result Of Demo Project:

1. 2-way bike lane: added temporary signage warning bicyclists on the 2-way bike lane to look for cars exiting/entering driveways, for cars to look both ways for bicyclists, and where 2-way bike lane ends to use the crosswalk.
2. Re-painting bike lane conflict areas with brighter green paint.
3. Adding solid green to start of bike lane with bike markings, to assure cars do not confuse bike lane for a car lane.
4. Adjusting delineators (aka – “armadillos”) to better allow U-turns.
5. Adjusting delineators (aka – “armadillos”) to better allow right turn at Pirie Rd, ValleRio Ave and into the Von’s driveway (between Carillo and Vons).
6. Reflectors added on lathe above planters to assure cars can see the planter as they backup or pullout.
7. Remove every other planter in parking areas to provide an easier entry and exit from parking. This increases the space between planters from 40 feet to 84 feet.

Demonstration Project Modifications for Consideration:

Recommended:

1. Providing more turn lanes or shoulder area turn into streets and commercial business; in particular the northbound right at Pirie.
2. Remove some delineators (aka – “armadillos”) to make parking maneuvering into parking spaces easier (allowing front tire to swing past parking stall limit to better maneuver into the parking stall).
3. Remove all parking south of Church Rd – there is no demand for parking in this area, so it is not needed. Some of the vacated parking could be used for right turning lanes, such as at the Ben Franklin driveway, at Carillo Rd. at the Holy Cross/medical driveway, and at the Medical Arts driveway.
4. Review “u-turn” locations and provide when possible, moving delineators if necessary.
5. Signage/Markings:
 - a. Add longer-term signage warning bicyclists on the 2-way bike lane to look for cars exiting/entering driveways, and for cars to look for bicyclists, and where 2-way bike lane ends.
 - b. Add longer-term signage warning cars that the bike lanes are for bikes only, as a few cars have driven down the bike lane, confusing it for a vehicle lane.
 - c. Add signage/markings clarifying bike lane is for bikes only at intersections to help clarify for car drivers.
 - d. Add signage and/or markings clarifying parking vs no parking (in diagonally striped areas), and school drop off lane. Note the permanent project currently proposes concrete curbed planters that will allow for permanent signage in these areas, which is not possible in the temporary project – see attachment B renderings.

Not Recommended:

1. Removing all planters - the planters provide an obvious physical barrier between the vehicle lane and the bike lane. Depending on only paint/markings is a less substantial and less protective. Not recommended at this time because although the temporary planters may not be attractive, they demonstrate the benefits of the proposed permanent concrete curbed planters with established landscaping that will be much more durable and attractive.

Permanent Project Modifications for Consideration: Staff will continue to observe during remainder of the demonstration project, as of now the following permanent modifications are being considered.

Recommended:

1. Widen the parking lane to allow easier maneuvering into and out of parking spaces, and more room between parked cars and passing cars. It appears there may be extra space available, dependent on Caltrans approval. Noting it will reduce the bike lane to 11 feet minimum, which will be sufficient for use as a detour lane. Also note the existing delineators (aka – “armadillos”) are not part of the final design, so there will not be a barrier between the parking stalls and the bike lane, providing more room for maneuvering.
2. Review u-turn locations and provide when possible.
3. Removing proposed parking south of Church Road (Church Rd to Y). This is also considered above in Interim Modifications. Parking at the Ojai Meadows is the primary demand for parking on Maricopa Hwy. There is some demand during specific timeframes for parking between the meadows and the Church Rd School driveway, demand outside of this area is unusual.
4. Expand 2-way bike lane: The Demonstration has a 2-way bike lane from Vallerio Ave to the High School’s Pirie Rd driveway entry, provided to allow school kids to ride from the housing tract at Vallerio to school without crossing the highway twice. Observations during the demonstration are showing that the lane is often used as 2-way even though not currently allowed and demand for a 2-way bike lane from the Y all the way to El Roblar Drive on the High school/Meadows side of the street. Depending on design, this may require an easement from OUSD and OVLC. If this option is supported, it is recommended a design be developed and a design exception submittal be made to Caltrans for permitting.

Not Recommended:

1. Removing all planters - the planters provide an obvious physical barrier between the vehicle lane and the bike lane. Depending on only paint/markings is a less substantial and less protective. (*not recommended*) Per above, note the temporary planters are admittedly not attractive. Keep in mind the permanent project proposes concrete curbed planters with established landscaping that will be much more durable and attractive.
2. Remove trees from planters – the trees provide a substantial permanent appearance, eventually providing shade to bikes, pedestrians and parked and moving cars. Trees are only planned on the southbound/Meadows side, as OVSD is opposed to tree roots near the sewer line on the northbound side.
3. Traffic Signals or HAWKS – there has been suggestions to add traffic signals (stop lights) at some of the intersections to provide stopped traffic for pedestrians. Similarly, to provide HAWK (High-intensity Activated crossWalk beacon) a traffic control device to stop traffic and allow pedestrians to cross safely – similar to the one installed on route 33 in Casitas Springs. These measures would require a full warrant study which is unlikely to be justified (*not recommended at this time*).

The above options are focused on modifications to the existing design, City Council may choose to discuss alternative designs if desired, such as revising the design to the pre-demonstration four lane configuration, with separated bike lanes along the curb.

Attachment D

ATP Project Cost Estimate

ATP Cost Estimate (2/1/2020)

	Pedestrian Improvements					Bike		Miscellaneous		Landscaping		Utilities	SUBTOTAL	
	Pedestrian Xing Improvements	Striping - (thermo-plastic)	Sidewalk Infill/ driveway/ ret walls/ curbs	Right of Way - Acquisition	Right of Way - Temporary Construction Easement	Bike Improvements Civil (curbs for trees, raised crossing, islands)	Striping - (thermo-plastic)	Striping (thermo-plastic)	General (TC, SWPPP, Staking, CM, CG)	Tree Infill	Non-Tree	Adjust/ Move Light poles, utility covers		
Maricopa Highway														
El Roblar to Church	\$ 63,111		\$ 21,677			\$ 59,445							\$ 522,643	
Church to Y	\$ 178,902		\$ 73,809	\$ 34,230	\$ 3,366	\$ 287,609							\$ 596,516	
NHS - Pire left turn median, plus remove median for east NHS park lot	-	\$ 5,860	\$ 97,238			-	\$ 63,613	\$ 68,037	\$ 182,000	\$ 39,300	\$ 19,600		\$ 97,238	
SUBTOTAL	\$ 242,013	\$ 5,860	\$ 192,724	\$ 34,230	\$ 3,366	\$ 347,054	\$ 63,613	\$ 68,037	\$ 182,000	\$ 39,300	\$ 19,600	\$ 18,600	\$ 1,216,397	
Category Subtotals:	Pedestrian Improvements Subtotal: \$ 478,193					Bike Subtotal: \$ 410,667	Misc Sub: \$ 250,037		Landscaping: \$ 58,900	\$ 18,600		\$ 1,216,397		
Ojai Ave														
Del Norte to CCD	\$ 13,770		\$ 66,846		\$ 540	-							\$ 11,350	\$ 559,927
CCD to Bristol	\$ 12,891		\$ 206,400	\$ 4,830	\$ 10,926	-							\$ 20,000	\$ 255,047
Bristol to Topa	\$ 25,882		\$ 326,473	\$ 6,750	\$ 20,568	-							\$ 49,800	\$ 429,473
Canada St/ El Paseo NW Curb	\$ 63,329		-	\$ 9,300		-							\$ 17,800	\$ 97,379
Canada St/ El Paseo NE Curb	\$ 23,357		-	\$ 2,940		-							\$ 450	\$ 26,747
Canada St/ El Paseo Median Island	-	\$ 21,760	-			\$ 18,945	\$ 85,626	\$ 87,035	\$ 273,000				\$ 3,850	\$ - 22,795
Blanche St to Signal St	\$ 74,744		-	\$ 14,400		-							\$ 12,150	\$ 111,694
Signal St to N. Montgomery	\$ 83,245		-	\$ 36,120		-							\$ 15,900	\$ 135,265
N. Montgomery to Bryant	\$ 289,236		-	\$ 2,760		-							\$ 6,500	\$ 298,496
Bryant to Shady	\$ 52,822		-	\$ 28,980		-							\$ 900	\$ 82,702
Shady to Gridley	-		\$ 329,911	\$ 81,000	\$ 45,828	-							\$ 2,300	\$ 459,039
SUBTOTAL	\$ 639,276	\$ 21,760	\$ 929,630	\$ 187,080	\$ 77,862	\$ 18,945	\$ 85,626	\$ 87,035	\$ 273,000	\$ 2,300	\$ 18,900	\$ 137,150	\$ 2,478,564	
Category Subtotals:	Pedestrian Improvements Subtotal: \$ 1,855,608					Bike Subtotal: \$ 104,571	Misc Sub: \$ 360,035		Landscaping: \$ 21,200	\$ 137,150		\$ 2,478,564		
TOTAL (Maricopa + Ojai Ave)	\$ 881,289	\$ 27,620	\$ 1,122,354	\$ 221,310	\$ 81,228	\$ 365,999	\$ 149,239	\$ 155,072	\$ 455,000	\$ 41,600	\$ 38,500	\$ 155,750	\$ 3,694,961	
Category Subtotals:	Pedestrian Improvements Subtotal: \$ 2,333,801					Bike Subtotal: \$ 515,238	Misc Sub: \$ 610,072		Landscaping: \$ 80,100	\$ 155,750		\$ 3,694,961		

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Attachment E

Survey Summary

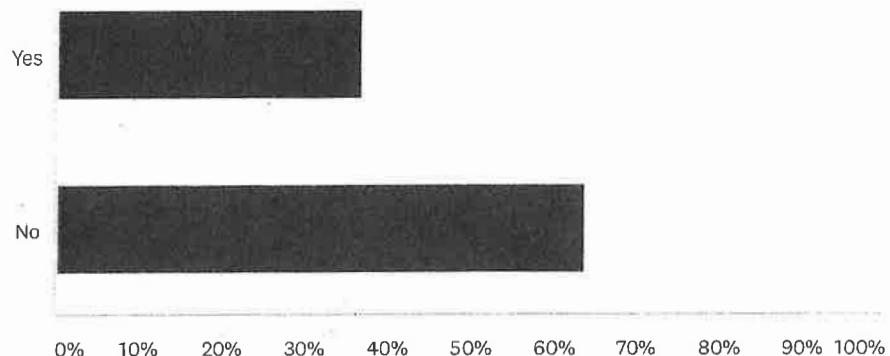
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Q1 What is your name and email?

Answered: 477 Skipped: 24

Q2 Do you live within a five minute walk of Maricopa between E Cuyama Rd. and Ojai Avenue?

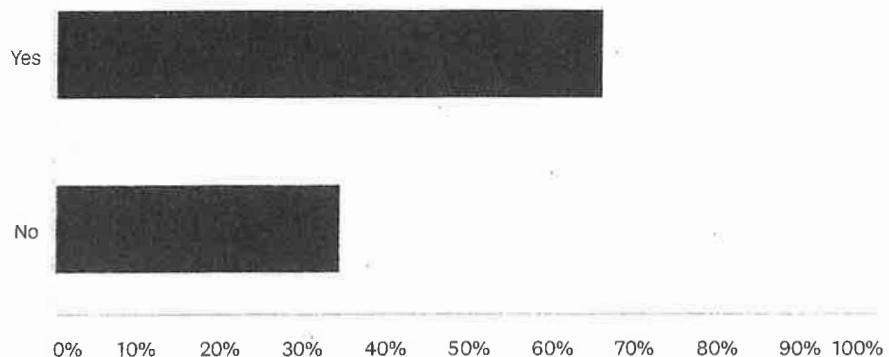
Answered: 491 Skipped: 10



ANSWER CHOICES	RESPONSES	
Yes	36.66%	180
No	63.34%	311
TOTAL		491

Q3 Do you live within a five minute bike ride of Maricopa between E Cuyama Rd. and Ojai Avenue?

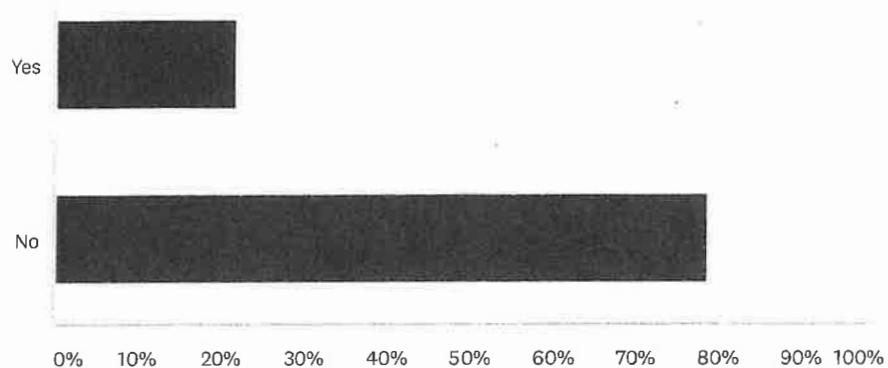
Answered: 494 Skipped: 7



ANSWER CHOICES	RESPONSES	
Yes	65.79%	325
No	34.21%	169
TOTAL		494

Q4 Do you work within a five minute walk of Maricopa between E Cuyama Rd. and Ojai Avenue?

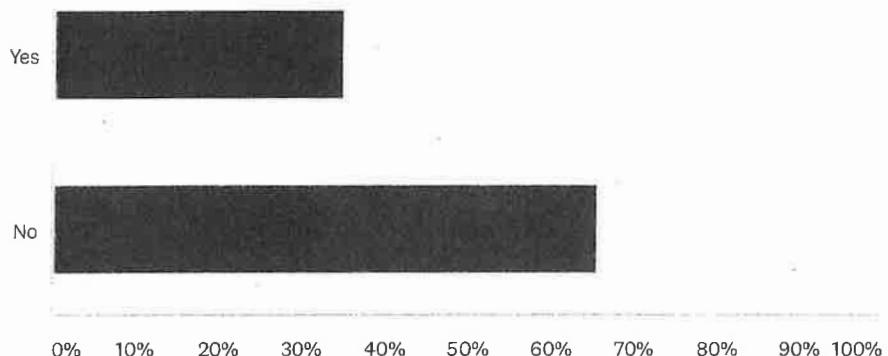
Answered: 484 Skipped: 17



ANSWER CHOICES	RESPONSES	
Yes	21.49%	104
No	78.51%	380
TOTAL		484

Q5 Do you work within a five minute bike ride of Maricopa between E Cuyama Rd. and Ojai Avenue?

Answered: 482 Skipped: 19



ANSWER CHOICES

Yes

No

TOTAL

RESPONSES

34.65%

167

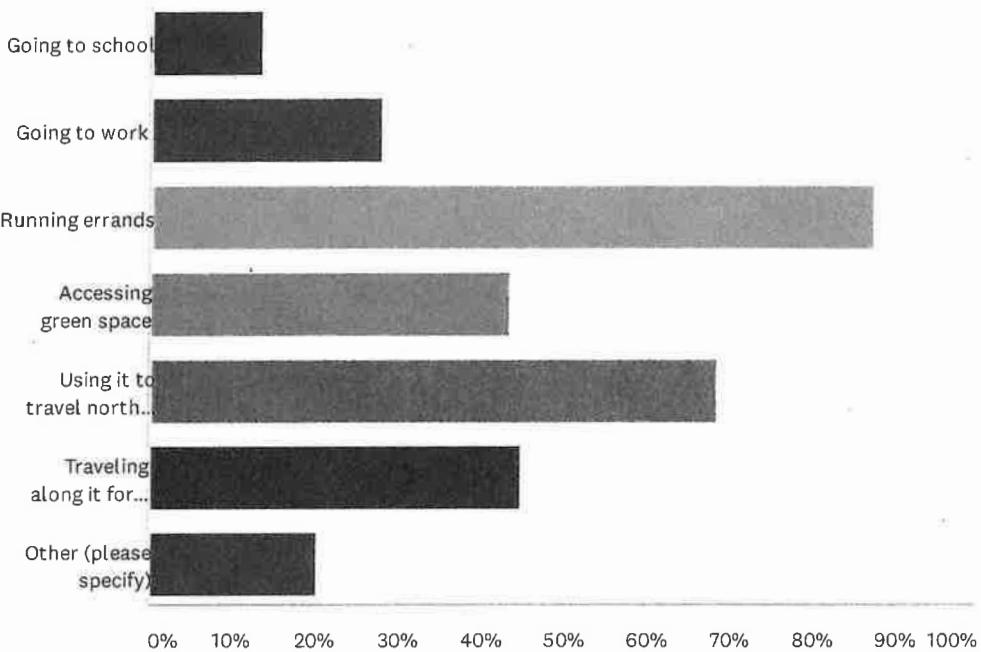
65.35%

315

482

Q6 When you travel along Maricopa Highway between E Cuyama Rd. and Ojai Avenue, where are you going/what is your purpose? You may select multiple.

Answered: 497 Skipped: 4



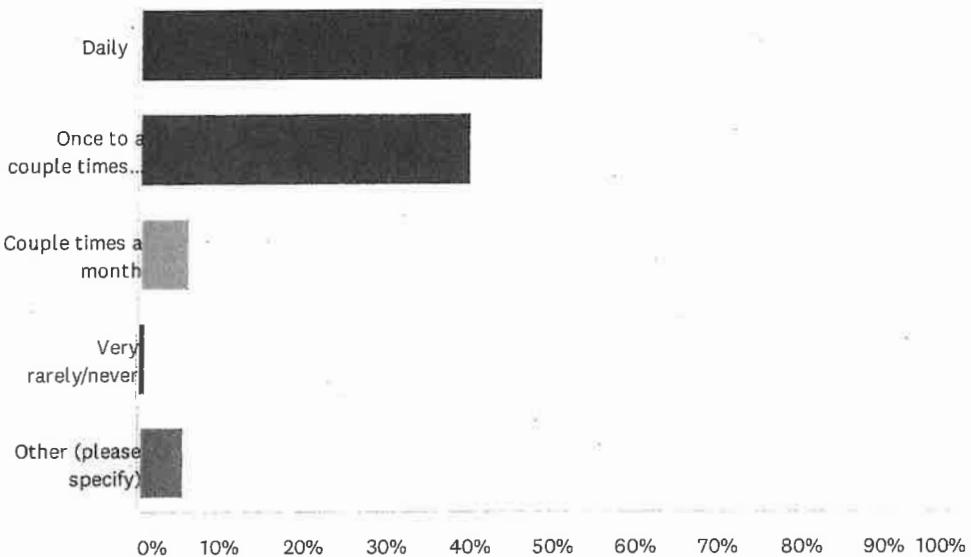
ANSWER CHOICES

RESPONSES

Going to school	13.28%	66
Going to work	27.57%	137
Running errands	86.72%	431
Accessing green space	43.06%	214
Using it to travel north or south elsewhere in or out of Ojai	68.01%	338
Traveling along it for leisure/as a part of leisure	44.47%	221
Other (please specify)	20.12%	100
Total Respondents: 497		

Q7 How often do you drive along Maricopa Highway between E Cuyama Rd. and Ojai Avenue?

Answered: 495 Skipped: 6



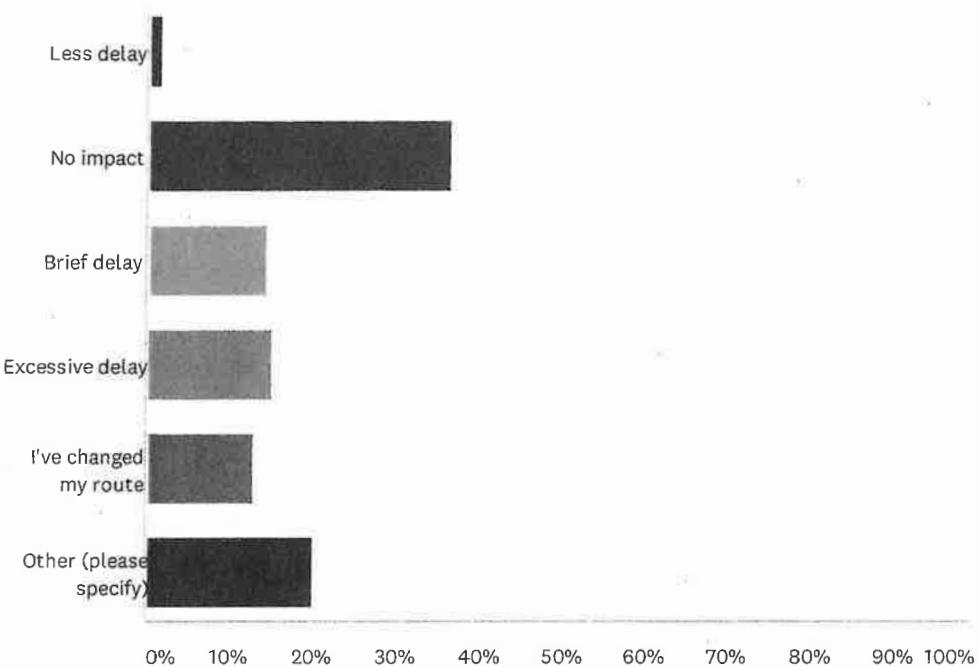
ANSWER CHOICES

RESPONSES

Daily	48.28%	239
Once to a couple times a week	39.80%	197
Couple times a month	5.86%	29
Very rarely/never	0.81%	4
Other (please specify)	5.25%	26
TOTAL		495

Q8 If and when you drive along Maricopa Highway between E Cuyama Rd. and Ojai Avenue, how has the ATP Demonstration Project impacted your drive?

Answered: 490 Skipped: 11

**ANSWER CHOICES**

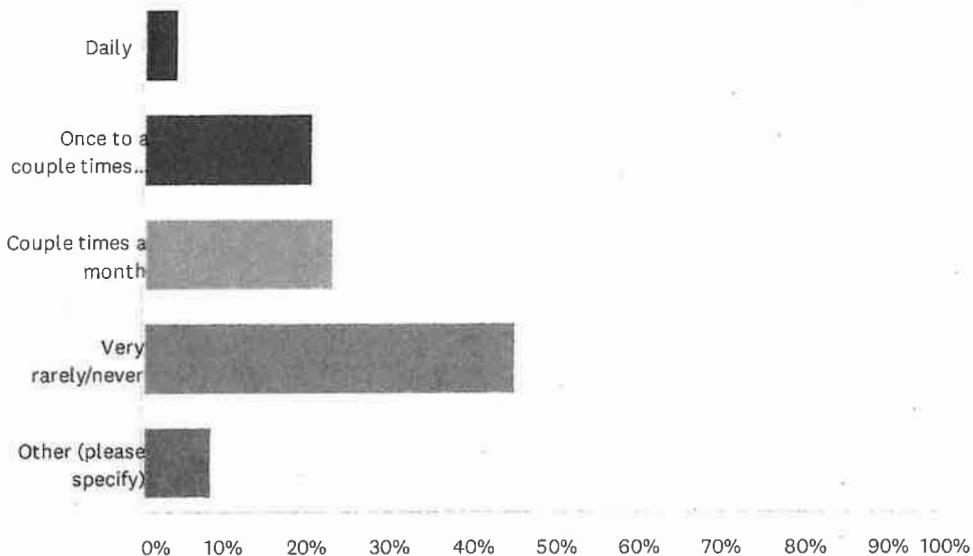
Less delay
No impact
Brief delay
Excessive delay
I've changed my route
Other (please specify)

RESPONSES

Less delay	1.43%	7
No impact	36.33%	178
Brief delay	14.29%	70
Excessive delay	15.10%	74
I've changed my route	12.86%	63
Other (please specify)	20.00%	98
TOTAL		490

Q9 How often do you ride a bike along Maricopa Highway between E Cuyama Rd. and Ojai Avenue?

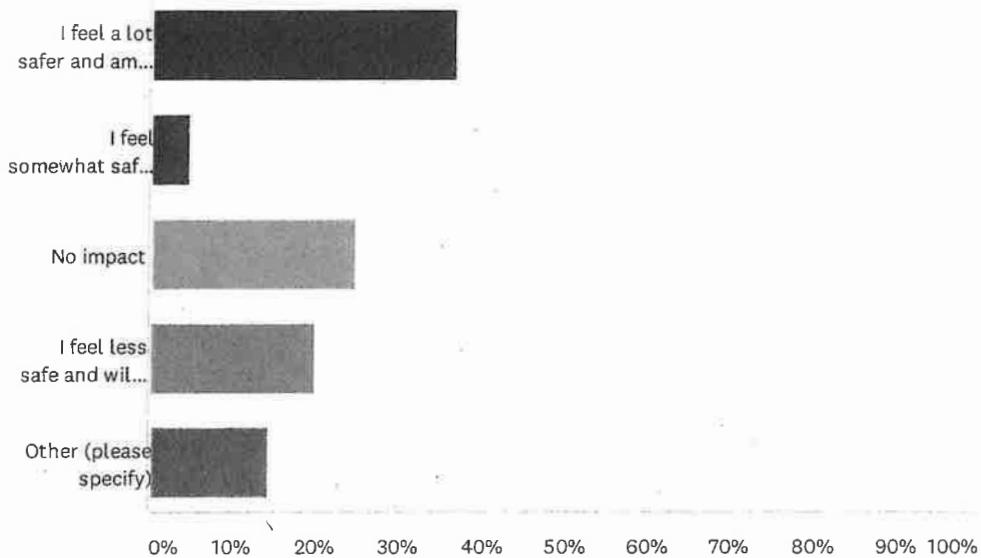
Answered: 492 Skipped: 9

**ANSWER CHOICES****RESPONSES**

Daily	4.07%	20
Once to a couple times a week	20.33%	100
Couple times a month	22.76%	112
Very rarely/never	44.72%	220
Other (please specify)	8.13%	40
TOTAL		492

Q10 If and when you ride a bike along Maricopa Highway between E Cuyama Rd. and Ojai Avenue, how has the ATP Demonstration Project impacted your ride?

Answered: 416 Skipped: 85



ANSWER CHOICES

I feel a lot safer and am more likely to ride my bike on Maricopa Highway.

36.54% 152

I feel somewhat safer and will continue to ride on Maricopa Highway.

4.57% 19

No impact

24.76% 103

I feel less safe and will avoid biking on Maricopa Highway.

19.95% 83

Other (please specify)

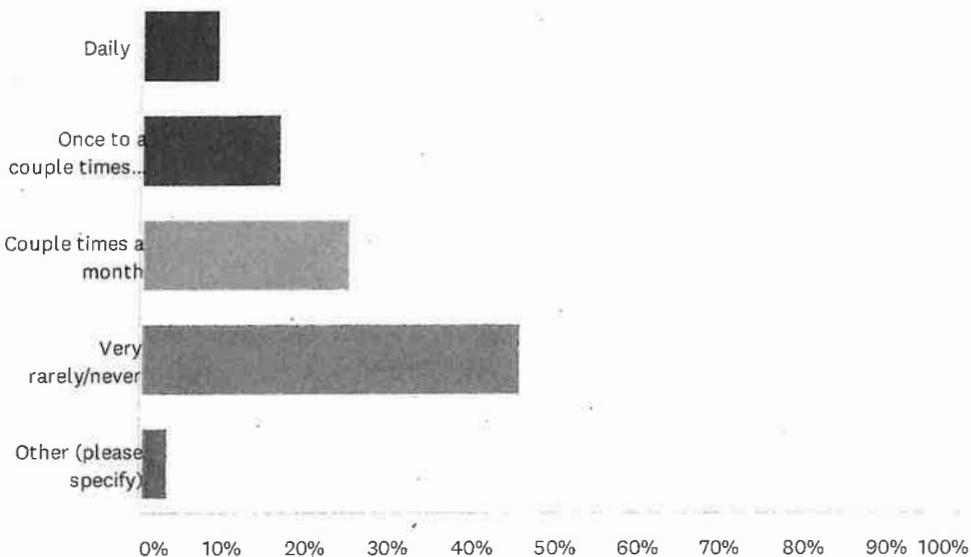
14.18% 59

TOTAL

416

Q11 How often do you walk along Maricopa Highway between E Cuyama Rd. and Ojai Avenue?

Answered: 488 Skipped: 13



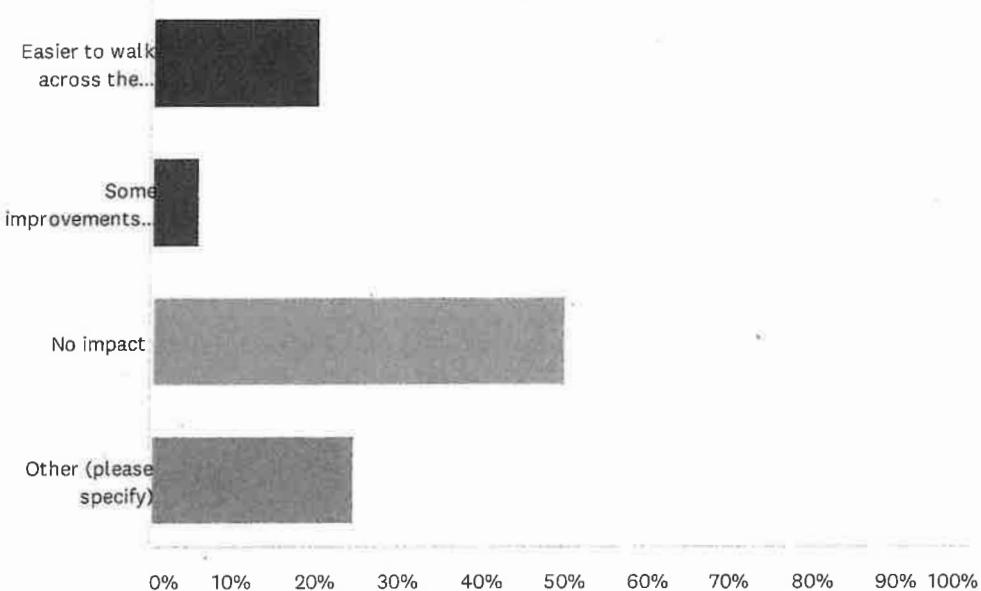
ANSWER CHOICES

RESPONSES

Daily	9.22%	45
Once to a couple times a week	16.80%	82
Couple times a month	25.20%	123
Very rarely/never	45.70%	223
Other (please specify)	3.07%	15
TOTAL		488

Q12 If and when you walk along Maricopa Highway between E Cuyama Rd. and Ojai Avenue, how has the ATP Demonstration Project impacted your walk?

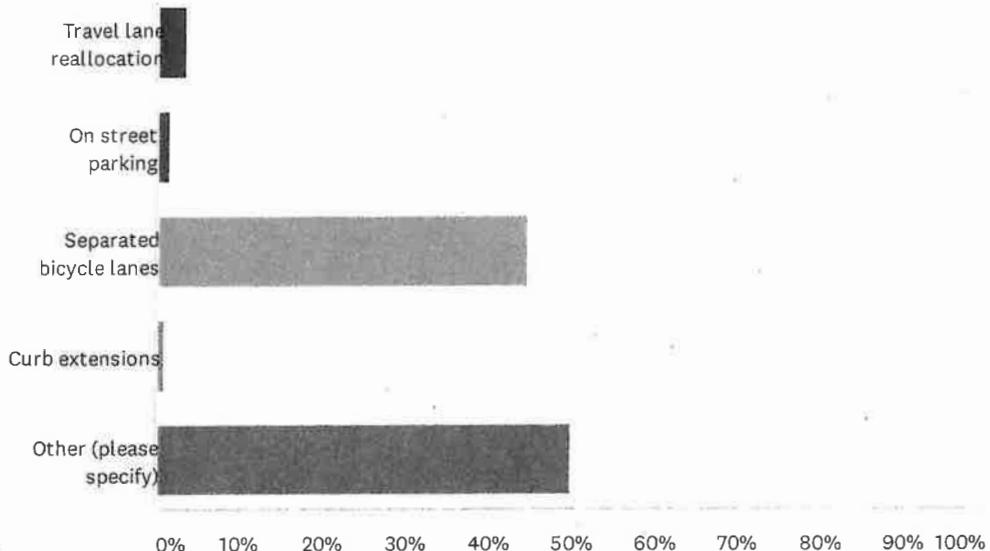
Answered: 426 Skipped: 75



ANSWER CHOICES	RESPONSES
Easier to walk across the street	20.19% 86
Some improvements for walking, but more needs to be done	5.63% 24
No impact	49.77% 212
Other (please specify)	24.41% 104
TOTAL	426

Q13 What has been the most successful element of the Maricopa Highway Demonstration Project?

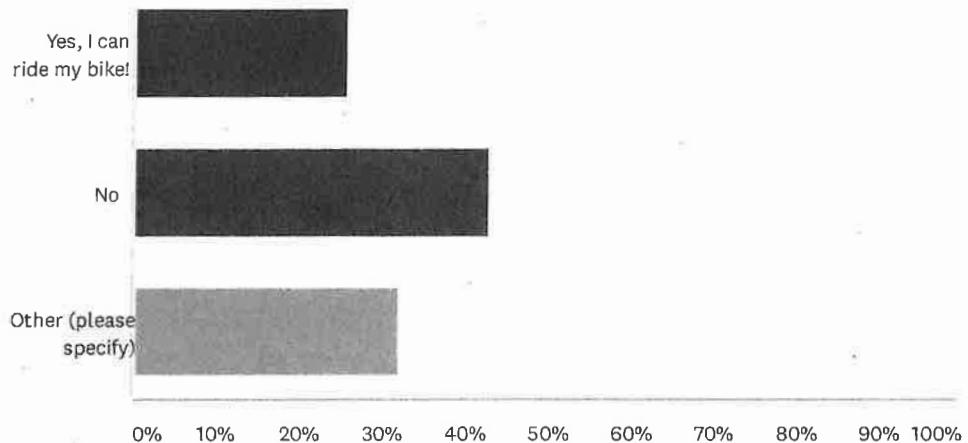
Answered: 456 Skipped: 45



ANSWER CHOICES	RESPONSES	
Travel lane reallocation	3.29%	15
On street parking	1.54%	7
Separated bicycle lanes	44.52%	203
Curb extensions	0.88%	4
Other (please specify)	49.78%	227
TOTAL		456

Q14 Has the addition of a protected bicycle lane along Maricopa Highway between E Cuyama Rd. and Ojai Avenue changed the way you travel along the corridor?

Answered: 488 Skipped: 13



ANSWER CHOICES

Yes, I can ride my bike!

No

Other (please specify)

TOTAL

RESPONSES

25.61%

125

42.62%

208

31.76%

155

488

Q15 Has the introduction of on-street parking along Maricopa Highway between E Cuyama Rd. and Ojai Avenue changed the way you utilize the corridor?

Answered: 478 Skipped: 23

No



If yes, how?



0% 10% 20% 30% 40% 50% 60% 70% 80% 90% 100%

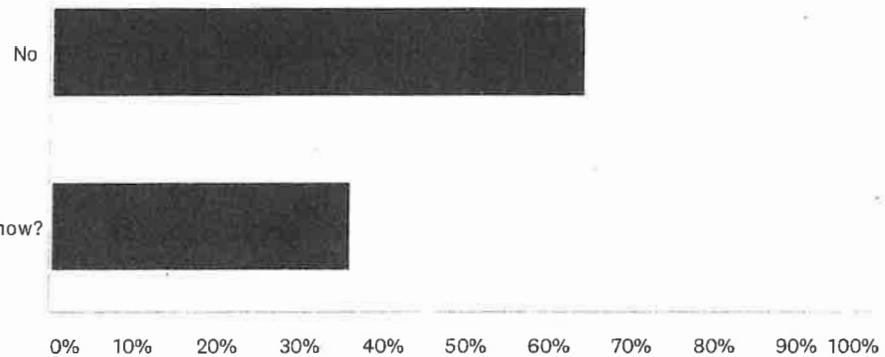
ANSWER CHOICES

RESPONSES

No	65.69%	314
If yes, how?	34.31%	164
TOTAL		478

Q16 Have the introduction of intersection treatments along Maricopa Highway between E Cuyama Rd. and Ojai Avenue changed the way you utilize the corridor?

Answered: 469 Skipped: 32



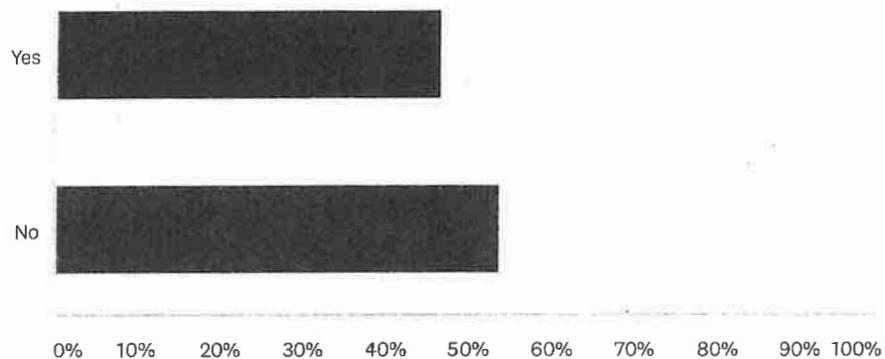
ANSWER CHOICES

RESPONSES

No	63.97%	300
If yes, how?	36.03%	169
TOTAL		469

Q17 Have you noticed any increased traffic on side streets as a result of the Demonstration Project?

Answered: 471 Skipped: 30



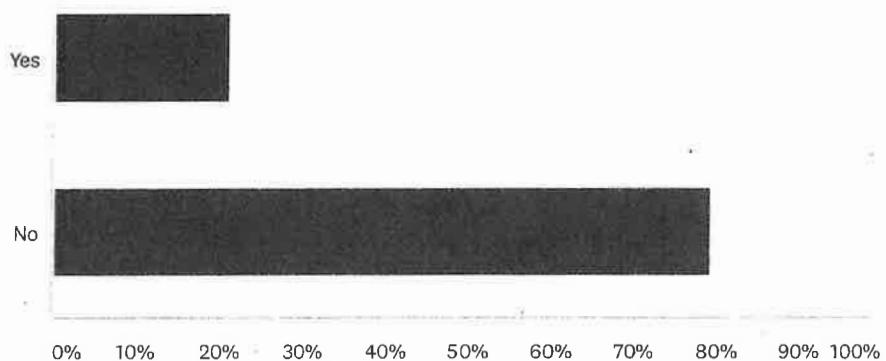
ANSWER CHOICES

RESPONSES

Yes	46.50%	219
No	53.50%	252
TOTAL		471

Q18 Have the interventions at the Church Rd. entrance to the school improved pick-up and drop-off, in terms of making it more efficient and/or safer to enter and exit?

Answered: 320 Skipped: 181



ANSWER CHOICES

Yes

No

TOTAL

RESPONSES

20.94%

67

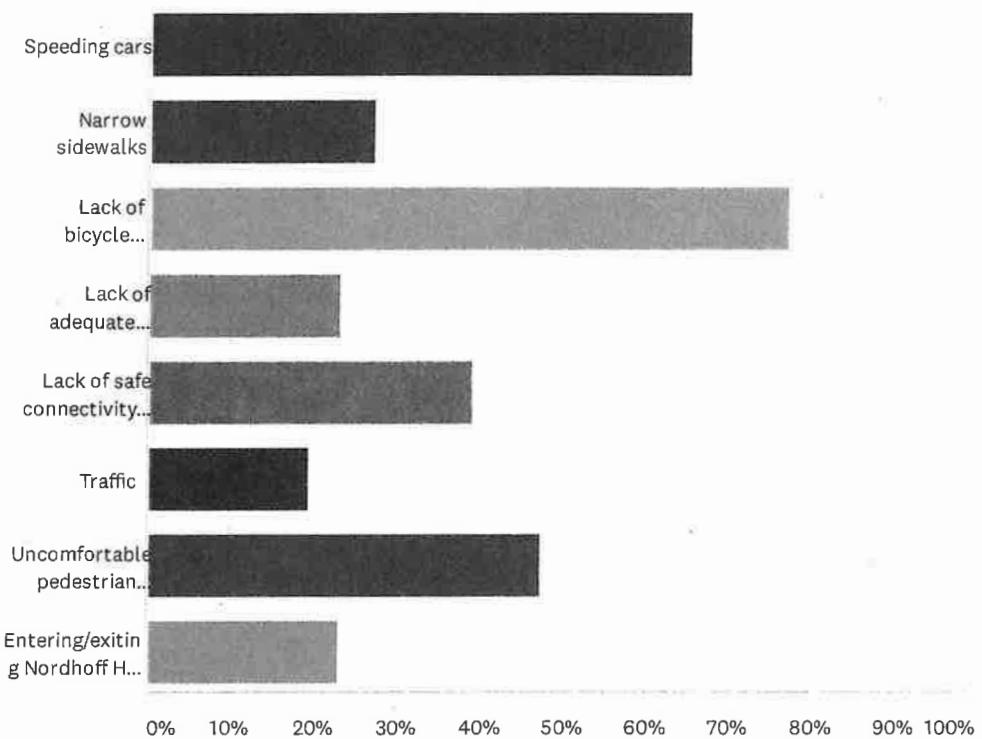
79.06%

253

320

Q19 Has the Demonstration Project addressed any of the below previously expressed challenges to traveling along Maricopa Highway between E Cuyama Rd. and Ojai Avenue? You may select multiple.

Answered: 286 Skipped: 215

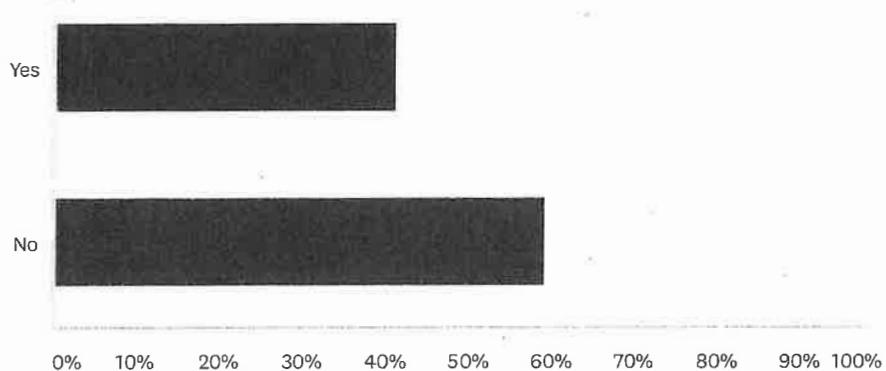
**ANSWER CHOICES****RESPONSES**

Speeding cars	65.03%	186
Narrow sidewalks	26.92%	77
Lack of bicycle infrastructure	76.92%	220
Lack of adequate parking	23.08%	66
Lack of safe connectivity to NHS or nearby open spaces	38.81%	111
Traffic	19.23%	55
Uncomfortable pedestrian crossings	47.20%	135
Entering/exiting Nordhoff High School	23.08%	66

Total Respondents: 286

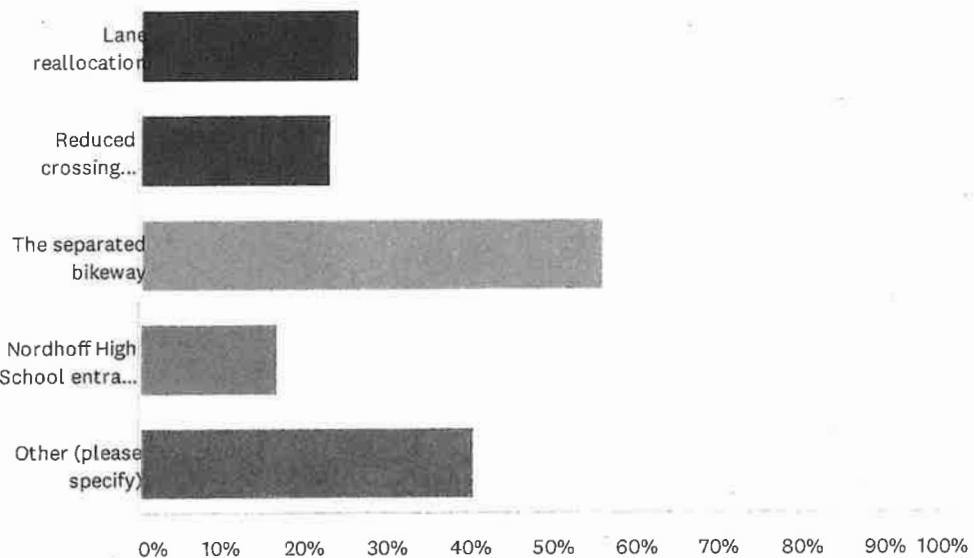
Q20 Do you support the City's ATP Project for this stretch of Maricopa Highway? Why?

Answered: 488 Skipped: 13



Q21 What elements of the permanent ATP project do you support the most? You may select multiple.

Answered: 413 Skipped: 88



ANSWER CHOICES

Lane reallocation
Reduced crossing distances at intersections
The separated bikeway
Nordhoff High School entrance improvements
Other (please specify)

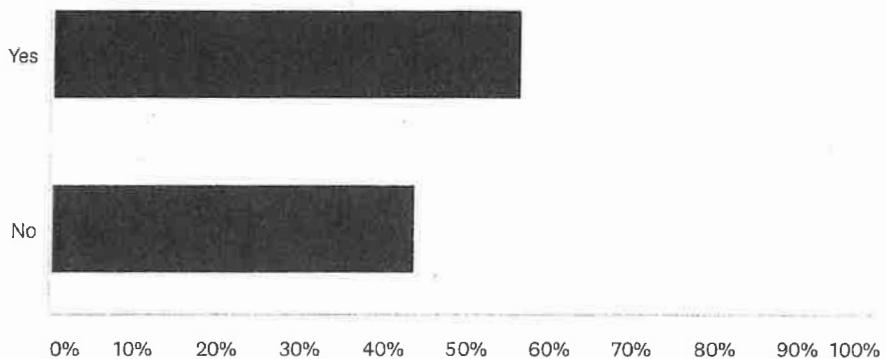
Total Respondents: 413

RESPONSES

Response	Percentage	Count
26.15%	108	
22.76%	94	
55.69%	230	
16.46%	68	
40.19%	166	

Q22 Do you support permanent bicycle and pedestrian improvements to Maricopa Highway, and why?

Answered: 474 Skipped: 27



Q23 Are there any other observations you'd like to share?

Answered: 335 Skipped: 166

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Attachment F

Public Comments Summary

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Public Comment Summary (Received for Meetings December 12, 2020 - January 26, 2021)

Email

Below is a summary of common issues in comments received, some are direct quotes. Full comments are available at: <https://ojaicity.org/public-comments/>.

Critique

Additional Signage/General Confusion

- Cars are not used to looking in the opposite direction of car travel for bikes. Cars are exiting the school at Pirie Rd. and are not aware of the bi-directional bikeway segment.
- What bikes are supposed to do when the two-way segment ends on either side is confusing.
- More signage is needed for people to understand what is bike space and what is car space.
- Delivery trucks are confused about where to pull in, if not all the way up to the curb.

Parking

- People parking close to traffic in the 8' parking lane is perceived as dangerous, especially when their doors are opening a little into the travel lane.
- Parallel parking will cause congestion.

Aesthetics

- "The green paint needs to be brighter."
- "It doesn't fit the character of Ojai."
- "Trees should be planted in the sidewalks, not in the middle of the road."

Increased Congestion

- Cars will be stopping traffic to make the turn at Church Rd.
- Concerned that Arbolada and Descanso traffic will increase as people try to avoid Maricopa.

Emergencies

- "Where are people supposed to pull over for emergency vehicles?"
- "There will be major traffic jams during school peak hours and large events."
- Concerns about the use of the emergency access lane.
- Concerns about impacts on emergency evacuations.

General Safety

- "It will make it more dangerous to pick up kids."
- Concern that bicyclists are more likely to be hit now at intersections when they are separated by a lane, because they were more easily seen when they were right next to cars.
- Concerns that seniors will have a hard time making left turns now with just one lane of traffic.
- Bicyclists and pedestrians could hit or trip on the delineators.

Maintenance

- "Who will maintain the project?"

Miscellaneous

- "Kids won't ride their bikes to school."
- Just a painted bike lane, no parking at all, with beacons that turn red to fully stop cars at crosswalks (flashing beacons don't work) would be better.
- Allow a u-turn at the Church Rd. school entrance.
- Put in stop lights at Church Rd. and Vallerio Ave.
- Consider lighting the edges of the bike lane at intersections.

Support

(all direct quotes)

- My sister and I are daily walkers at the Meadows as well as doing most of our local errands along that road and we love it. We feel safer both driving and walking.
- I live in the city of Ojai (~20 years) and am currently on the board for the Ojai Fire Safe Council. I volunteered to assist with the setup on the ATP during the initial implementation. I continue to believe in this project as an improvement to our city's infrastructure.
- I love the redesign. It's reduced the ability to speed through the school zone anymore and lives will be saved in the long run. I never understood why a four-lane road was needed through that little stretch anyway. It's not any busier than anywhere else in the valley and it's the only one for miles. In my humble opinion, any resistance to it can be put down to our inability to accept a change from what we've grown used to, exacerbated by these uncertain times.
- I can't wait to start using it to bike downtown from Meiners Oaks. I've been looking for ways to avoid that stretch to get to the main bike trail.
- I love the design - we live in the neighborhood and are daily users of the Meadows. We feel safer. Thank you for your work on this.
- I don't know why people have been complaining about the project. I think that with COVID so much has been out of control that they are reacting strongly to any changes in anything. This project has been well-conceived and executed beautifully.
- I think the new lanes and planters are beautiful and I love that I get to live in a place that prioritizes the safety of all of its residents, not just the ones who view the world from the inside of a metal cage with wheels. I can't wait to take another gasoline-free ride to the Preserve soon. Thanks to all the volunteers who made this happen!
- We're talking about a 3/4 mile stretch of road with a ton of bicycle and pedestrian traffic from Meiner's Oaks to the Ojai Valley Trail and Ojai with no other safe option - bicyclists will no longer have to chance it on Cuyama, where I'll remind you, someone was just killed by a driver earlier this summer - and they'll have a SAFE, buffered bike lane. And what a great improvement for kids traveling to and from the school there.

Attachment G

Incidents

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Incidents:

Bicyclist accident:

- 12/13: a bicycle accident occurred at the Nordhoff High School Church Road driveway exit. The electric bike with light was traveling north on the southbound side of the street (opposite normal flow), a block south of the end of the 2-way bike lane (a 2-way bike lane was provided from ValleRio Ave to the High School so students would not have to cross the street twice for the small distance). In response, temporary signage notifying bicyclists of the end of the 2-way bike lane at the School's Pirie Drive driveway, and signage cautioning cars and bikes of cars/bikes on the 2-way section has been added. This signage will be considered further for a more permanent fix during the demonstration project as well as the permanent project.

Planter damage:

- 12/24: At approximately 7:00am, 11 planters were hit near the Ojai Meadows Preserve, by a driver driving on a suspended driver's license who was apprehended and arrested.
- 12/25: 7 planters appeared to be intentionally hit in the early morning in various locations between Church Rd and El Roblar Dr.
- 12/27: 2 planters appeared to be intentionally hit in the early morning between Church Rd and El Roblar Dr.
- There have been various planters damaged by parked cars pulling in and out of parking by the Meadows Preserve. In response every other planter has been removed along this area and reflectors have been placed above the planters to make them more visible.

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Attachment H

ATP Demonstration Project Plans

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INDEX OF PLANS

SHEET

NO. DESCRIPTION

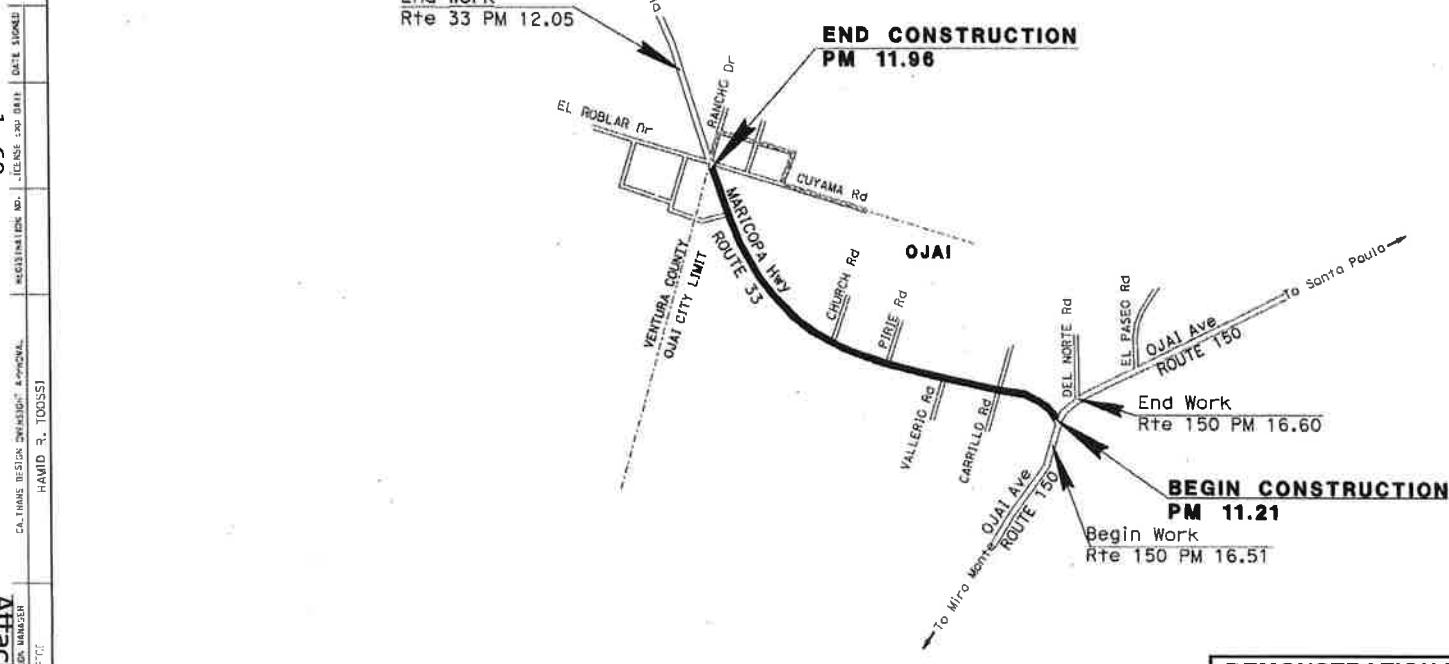
- 1 TITLE AND LOCATION MAP
- 2-15 PAVEMENT DELINEATION AND SIGN PLANS AND DETAILS
- 5-19 NUMINA SENSOR LOCATIONS

STATE OF CALIFORNIA
DEPARTMENT OF TRANSPORTATION
PROJECT PLANS FOR CONSTRUCTION ON
STATE HIGHWAY
IN VENTURA COUNTY
IN THE CITY OF OJAI
FROM ROUTE 150 TO EL ROBLAR DRIVE

TO BE SUPPLEMENTED BY STANDARD PLANS DATED 2018



69-1



NO SCALE

DEMONSTRATION PROJECT
CALTRANS DISTRICT 7
PERMIT No. 720-AOP-2411



GENERAL DRAWING NOTES

1. GRAY STRIPING INDICATES EXISTING STRIPING ALONG MARICOPA HWY
2. RED STRIPING INDICATES EXISTING STRIPING TO BE REMOVED FOR DEMONSTRATION, AND WILL BE PUT BACK IN PLACE AT THE END OF THE DEMONSTRATION
3. GREEN CONFLICT ZONE MARKINGS LOCATED AT ALL DRIVEWAYS AND INTERSECTIONS (SEE DETAIL TO THE RIGHT)
4. DRIVEWAYS & INTERSECTIONS ARE STANDARDIZED WITH ROUNDED CURB EXTENSIONS. ALL CURB RADII RANGING FROM 15' TO 25'
5. FOR PARKING SPACES IN LOCATIONS WITH A CURB RADIUS OF 15', A TANGENT LENGTH OF 20' MIN. WILL BE PROVIDED - FOR LOCATIONS WITH A CURB RADIUS OF 25', A TANGENT LENGTH OF 50' MIN. WILL BE PROVIDED TO ACCOMMODATE NECESSARY SIGHT DISTANCE
6. 2' GUTTER IS FROM SURVEY PROVIDED BY PERMANENT PROJECT CONSULTANT, AND FIELD VERIFIED 6/13/2020 BY CITY OF OJAI DEPARTMENT OF PUBLIC WORKS.
7. DELINEATORS THAT CONFLICT WITH MANHOLE/SEWER ACCESS TO BE VERIFIED IN-FIELD AND ADJUSTED DURING IMPLEMENTATION AS NECESSARY

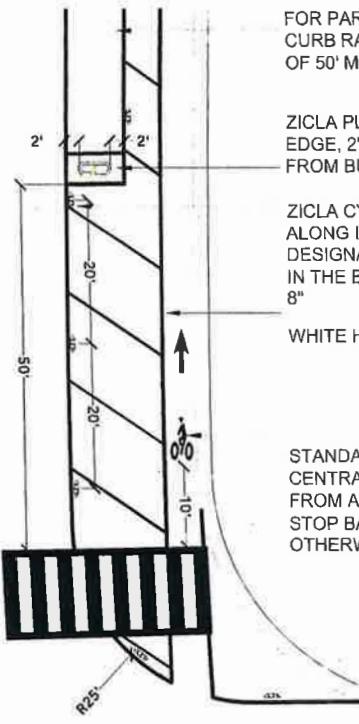
MATERIAL SPECIFICATIONS

2. BIKE LANE/PARKING STRIPING: WHITE HOTLINE TRAFFIC PAINT 6" WIDE (SEE PDF PAGES 24-33)
3. CONFLICT ZONE MARKINGS: GREEN HOTLINE TRAFFIC PAINT
4. CLOSING TURN LANES: YELLOW HOTLINE TRAFFIC PAINT 6" WIDE
5. DELINEATORS: ZICLA CYCLE LANE SEPARATOR (SEE PAGES S15-S17 FOR DETAILS)
6. PLANTERS IN THE PARKING LANE: ZICLA PLANTER (SEE PAGES S15-S17 FOR DETAILS)
7. PROJECT INFORMATION BANNERS TO BE ATTACHED TO EXISTING LIGHT POLES WHERE INDICATED ON THE PLAN (SEE PAGES S18-S19 FOR SPECIFICATIONS)
8. MUTCD COMPLIANT SIGNS TO BE AFFIXED TO EXISTING POLES

Attachment H
Page 2 of 15

PAVEMENT MARKING DETAILS

TYPICAL BIKE LANE EGRESS FROM A SIDE STREET DETAIL



FOR PARKING SPACES IN LOCATIONS WITH A CURB RADIUS OF 25', A TANGENT LENGTH OF 50' MIN. IS BE PROVIDED

ZICLA PLANTERS (12" X 40") SPACED 40' EDGE TO EDGE, 2' AWAY FROM TRAVEL LANE AND 2' AWAY FROM BUFFER.

ZICLA CYCLE LANE SEPARATOR SPACED 20' O.C.
ALONG INSIDE EDGE OF WHITE STRIPING EXCEPT AT
DESIGNATED CURB EXTENSIONS ON S5 AND S7, AND
IN THE BUFFER ON S3 AND S5. DELINEATOR WIDTH IS
8"

WHITE HOTLINE TRAFFIC PAINT

**STANDARD BIKE SYMBOL TO BE PLACED
CENTRAL TO WIDTH OF BIKE LANE AND 10'
FROM ANY INTERSECTION, CROSSWALK, OR
STOP BAR AND SPACED 200' APART UNLESS
OTHERWISE NOTED**

TYPICAL CONFLICT ZONE MARKING

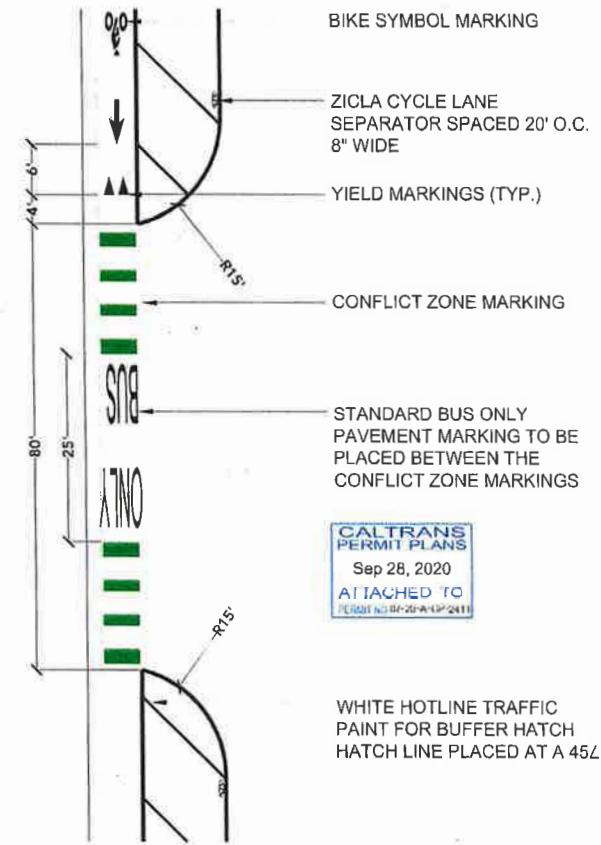


BIKE SYMBOL / YIELD MARKINGS

DIRECTIONAL ARROW



TYPICAL BUS STOP DETAIL



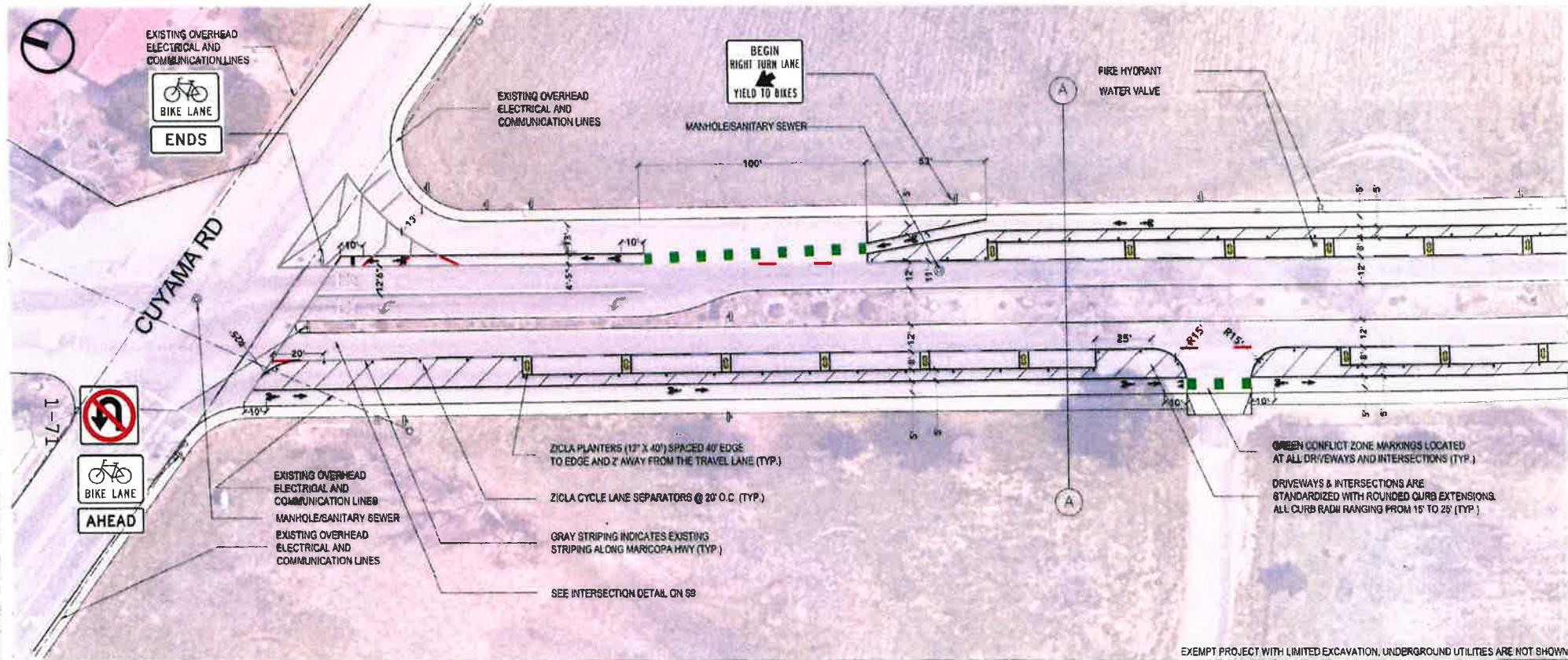
DEMONSTRATION PROJECT
CALTRANS DISTRICT 7
PERMIT # 720-AOP-2411

CONTEXT MAP



S1	MARICOPA HWY STRIPING PLAN
DRAWING TITLE	
REVIEWS	SUBMISSIONS
#1 04/01/2020	#1 04/02/2020 03 05/10/2020
#2 04/05/2020	#2 04/10/2020 04 08/12/2020 (FINAL)
N/A	

SHEET INDEX	
PAGE #	SHEET DESCRIPTION
S1.	GENERAL NOTES
S2-S8	STRIPPING DRAWINGS
S9-S14	INTERSECTION DETAIL DRAWINGS
S15-PDF PG 33	MATERIAL SPECIFICATIONS



DEMONSTRATION PROJECT SITE PLAN

DEMONSTRATION PROJECT
CALTRANS DISTRICT 7
PERMIT # 720-AOP-2411

Attachment
Page 3 of 15
STREET PLANS
STREET PLANS

S2 MARICOPA HWY STRIPING PLAN

DRAWING TITLE

REVIEWS SUBMISSIONS

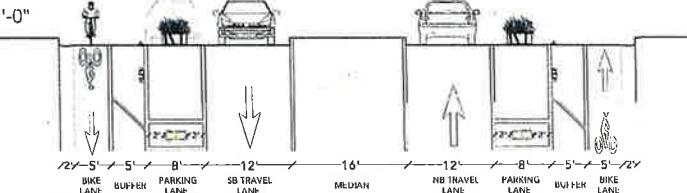
4/16/2020 4/16/2020
4/20/2020 4/20/2020
4/20/2020 4/20/2020

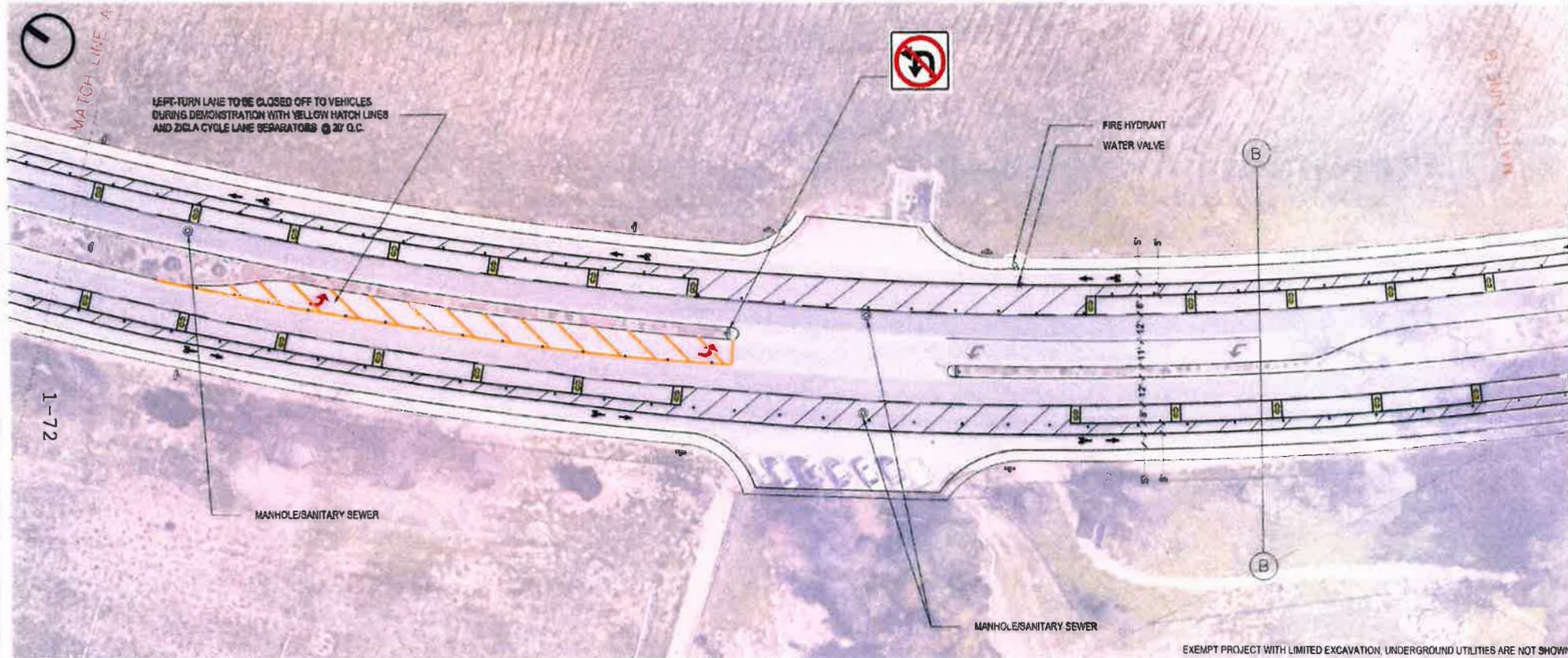
1:20 SCALE

NOTES

- EXISTING STRIPING AND PAVEMENT MARKINGS ARE SHOWN IN GRAY. STRIPING AND PAVEMENT MARKINGS SHOWN IN RED, WHICH CONFLICT WITH THE PROPOSED BIKE LANE, ARE TO BE REMOVED DURING DEMONSTRATION.
- PROPOSED BIKE LANE MARKINGS SHOWN IN BLACK, AND CONFLICT ZONE MARKINGS IN GREEN.
- STRIPING, WHITE GILMER WILLIAMS HOTLINE PAINT.
- DELINERATORS, ZICLA CYCLE LANE SEPARATOR, PLANTERS, ZICLA ZEBRA PLANTER (SEE PDF PAGES S15-S17).
- SIGNAGE, NUTCD SIGNS TO BE AFFIXED TO EXISTING POLES WHERE INDICATED, PROJECT BANNERS TO BE AFFIXED TO EXISTING LIGHT POLES WHERE INDICATED (SEE PDF PAGE 19 FOR MORE DETAILS).

STREET SECTION 'A'





DEMONSTRATION PROJECT SITE PLAN

DEMONSTRATION PROJECT
CALTRANS DISTRICT 7
PERMIT # 720-AOP-2411

Page 4
Attachment
STREET PLANS

15 OF STREET PLANS INC.
HAROLD H. TROSTLE, JR., PRESIDENT
305 SUNSET DRIVE, SUITE 4
MELBOURNE, FL 32934
305 878 0411 STREETPLANS.ORG

S3 MARICOPA HWY
STRIPING PLAN
DRAWING TITLE

REVIEWERS SUBMISSIONS

#1 06/21/2020 #1 07/13/2020

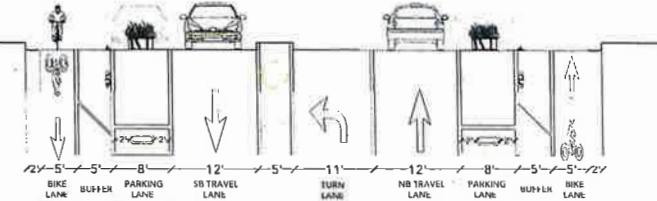
#2 04/30/2020 #3 07/18/2020 #4 08/21/2020 [PUBLISHED]

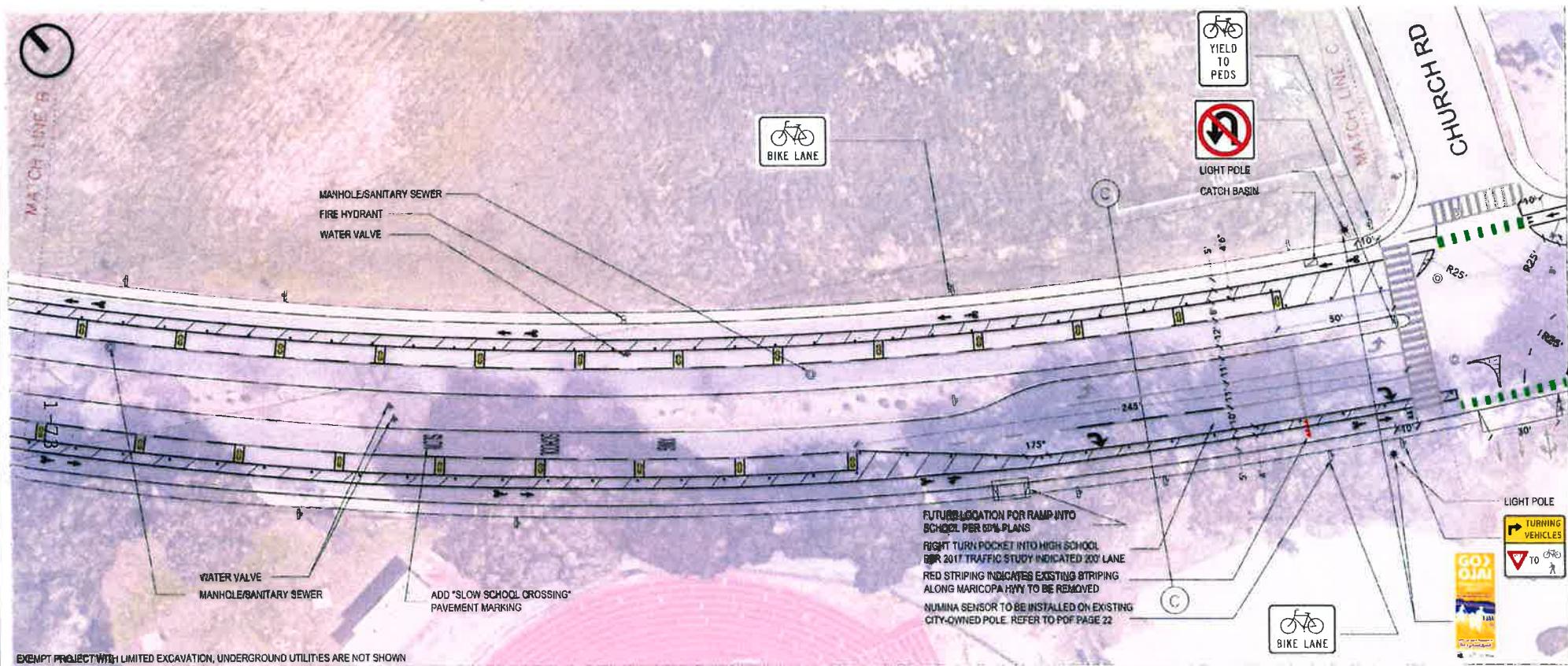
#3 05/17/2020 #5 08/17/2020 [HIGH CERT.]

NOTES

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2. PROPOSED BIKE LANE MARKINGS SHOW IN BLACK, AND CONFLICT ZONE MARKINGS IN GREEN.
3. STRIPING: WHITE SHERWIN WILLIAMS HOTLINE PAINT
4. DELINATEORS: ZICLA CYCLE LANE SEPARATOR, PLANTERS: ZICLA ZEBRA PLANTER (SEE S15-S17)
5. SIGNAGE: MUTCD SIGNS TO BE AFFIXED TO EXISTING POLES WHERE INDICATED, PROJECT BANNERS TO BE AFFIXED TO EXISTING LIGHT POLES WHILE INDICATED (SEE PDF PAGE 19 OR MORE DETAILS).

STREET SECTION 'B'





Attachment H
Page 5 of 15
SHEETPLANS

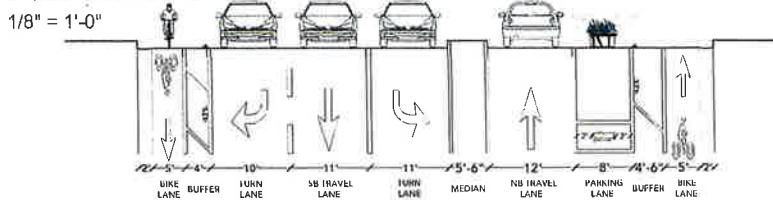
S4 MARICOPA HWY
STRIPING PLAN

REVIEWS	SUBMISSIONS
#10407308 #2100232002 #2100232002	#300232002 #400232002 (FINAL) #500232002 (URGENT CERT)
1/20 SCALE	

NOTES

- EXISTING STRIPING AND PAVEMENT MARKINGS ARE SHOWN IN GRAY. STRIPING AND PAVEMENT MARKINGS SHOWN IN RED, WHICH CONFLICT WITH THE PROPOSED BIKE LANE, ARE TO BE REMOVED DURING DEMONSTRATION.
- PROPOSED BIKE LANE MARKINGS SHOW IN BLACK, AND CONFLICT ZONE MARKINGS IN GREEN.
- STRIPING: WHITE SHERWIN WILLIAMS HO LINE PAINT
- DELINATEATORS: ZICLA CYCLE LANE SEPARATOR, PLANTERS, ZICLA ZEBRA PLANTER (SEE S15-S17)
- SIGNAGE: MUTCD SIGNS TO BE AFFIXED TO EXISTING POLES WHERE INDICATED, PROJECT BANNERS TO BE AFFIXED TO EXISTING LIGHT POLES WHERE INDICATED (SEE PDF PAGE 10 FOR MORE DETAILS)

STREET SECTION 'C'





EXEMPT PROJECT WITH LIMITED EXCAVATION, UNDERGROUND UTILITIES ARE NOT SHOWN

DEMONSTRATION PROJECT SITE PLAN

DEMONSTRATION PROJECT
CALTRANS DISTRICT 7
PERMIT # 720-AOP-2411

Attn:
Page 6 of 15
STREET PLANS
STREET PLANS INC
PLANNING, COORDINATION, DESIGN - RESEARCH
100 SUNSET DRIVE, SUITE 4,
SOUTH MIAMI, FL 33143
407.978.9426 | STREETPLANS.ORG

S5 MARICOPA HWY
STRIPING PLAN

DRAWING TITLE

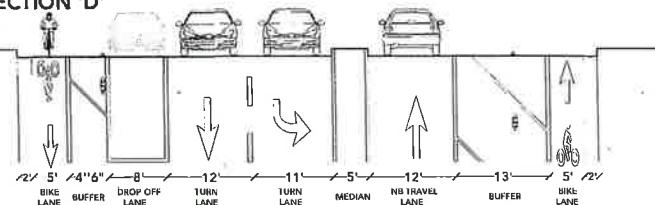
REVIEWS	SUBMISSIONS
#10407/2020	#11062/2020
#2/04/2020	#2/05/2020
#3/07/2020 (FINAL)	
#4/09/2020 (REV CNT)	

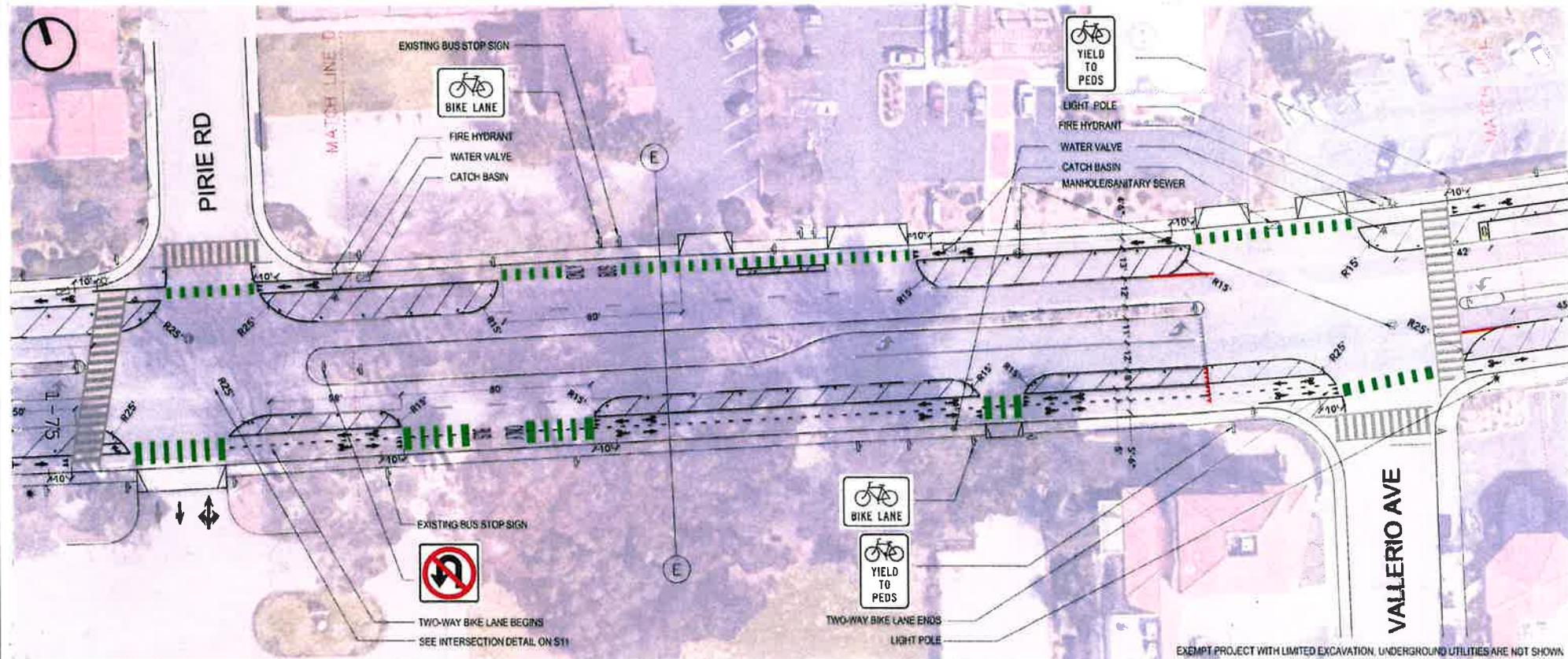
SCALE: 1:20

NOTES

1. EXISTING STRIPING AND PAVEMENT MARKINGS ARE SHOWN IN GRAY. STRIPING AND PAVEMENT MARKINGS SHOWN IN RED, WHICH CONFLICT WITH THE PROPOSED BIKE LANE, ARE TO BE REMOVED DURING DEMONSTRATION.
2. PROPOSED BIKE LANE MARKINGS SHOW IN BLACK, AND CONFLICT ZONE MARKINGS IN GREEN.
3. STRIPING: WHITE SHERWIN WILLIAMS HO LINE PAINT
4. DELINEATORS: ZICLA CYCLE LANE SEPARATOR, PLANTERS: ZICLA ZEBRA PLANTER (SEE S15-S17)
5. SIGNAGE: MUTCD SIGNS TO BE AFFIXED TO EXISTING POLES WHERE INDICATED, PROJECT BANNERS TO BE AFFIXED TO EXISTING LIGHT POLES WHERE INDICATED (SEE PDF PAGE 19 FOR MORE DETAILS)

STREET SECTION 'D'





DEMONSTRATION PROJECT SITE PLAN

**DEMONSTRATION PROJECT
CALTRANS DISTRICT 7
PERMIT # 720-AOP-2411**

**Attachment
Page 7 of 15**

S6	MARICOPA HWY STRIPING PLAN
DRAWING TITLE	
REVIEWS	SUBMISSIONS
#134037000	#1388252-23
#134047000	#12810102002
	#1388252-23
	#1388252-000
120	

NOTES

1. EXISTING STRIPING AND PAVEMENT MARKINGS ARE SHOWN IN GRAY. STRIPING AND PAVEMENT MARKINGS SHOWN IN RED, WHICH CONFLICT WITH THE PROPOSED BIKE LANE, ARE TO BE REMOVED DURING DEMONSTRATION

2. PROPOSED BIKE LANE MARKINGS SHOW IN BLACK, AND CONFLICT ZONE MARKINGS IN GREEN

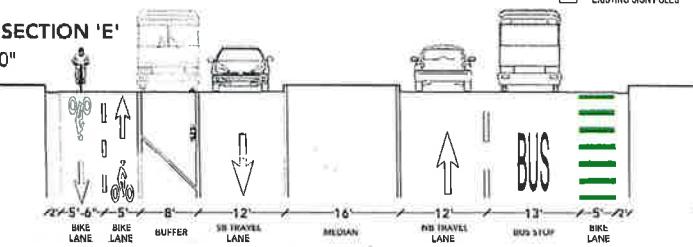
3. STRIPING: WHITE SHERWIN WILLIAMS HOTLINE PAINT

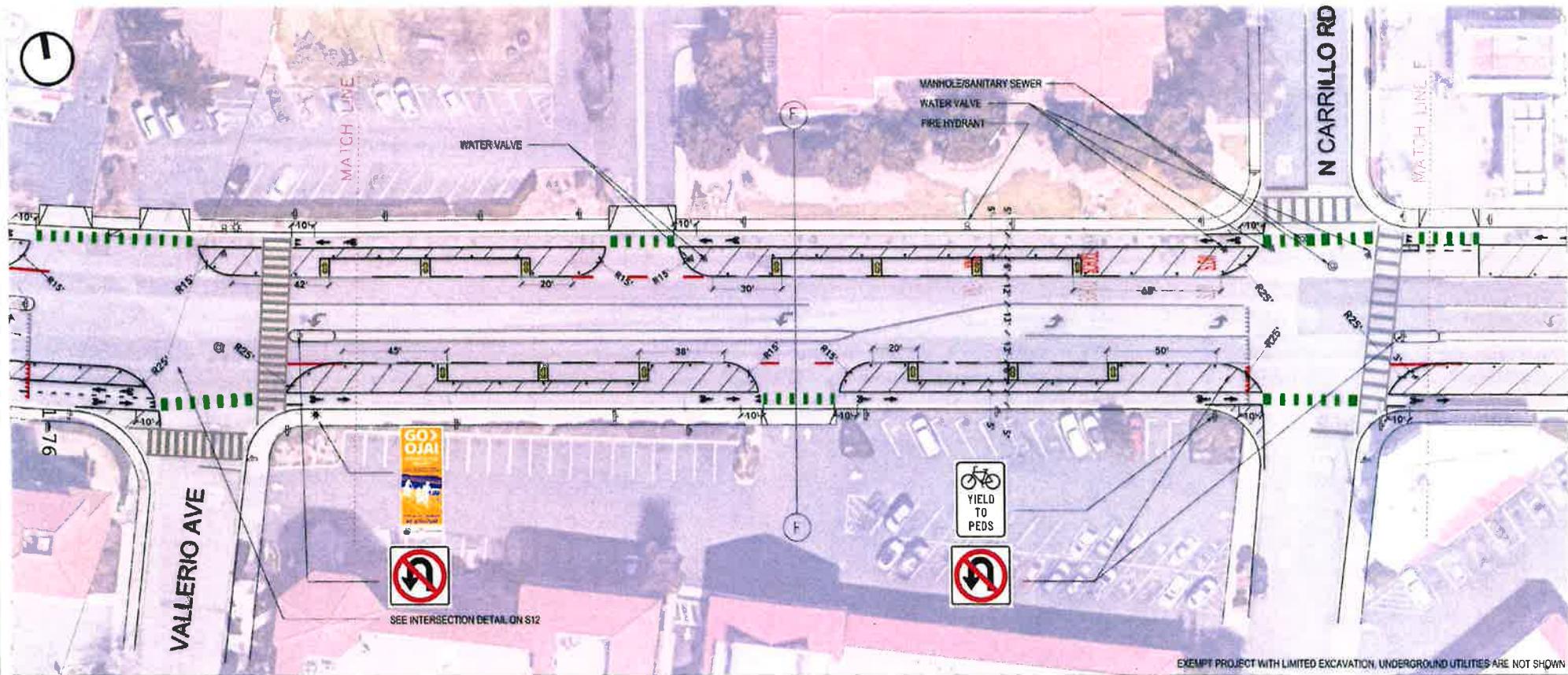
4. DELINERATOR: ZICLA CYCLE LANE SEPARATOR, PLANTER: ZICLA ZEBRA PLANTER (SEE BDF-SR05 1b-17)

5. SIGNAGE: MUTCD SIGNS TO BE AFFIXED TO EXISTING POLES WHERE INDICATED. PROJECT BANNERS TO BE AFFIXED TO EXISTING LIGHT POLES WHERE INDICATED (SEE PDD PAGE 19 FOR MORE DETAILS)

STREET SECTION 'E'

$$1/8" = 1'-0"$$





DEMONSTRATION PROJECT SITE PLAN

DEMONSTRATION PROJECT
CALTRANS DISTRICT 7
PERMIT # 720-AOP-2411

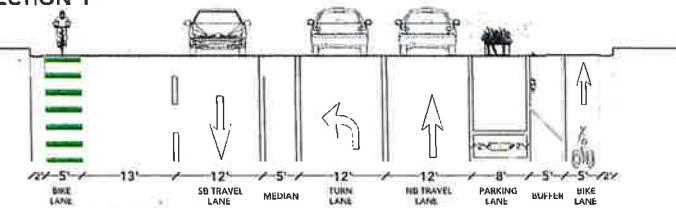
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Page 8 of 15
STREETPLANS
STREET PLANS INC
PLANNING, TRANSPORTATION, DESIGN, RESEARCH
305 SUNSET DRIVE, SUITE 4,
MURRIETA, CALIFORNIA 92564
305.070.6220 | STREETPLANS.ORG

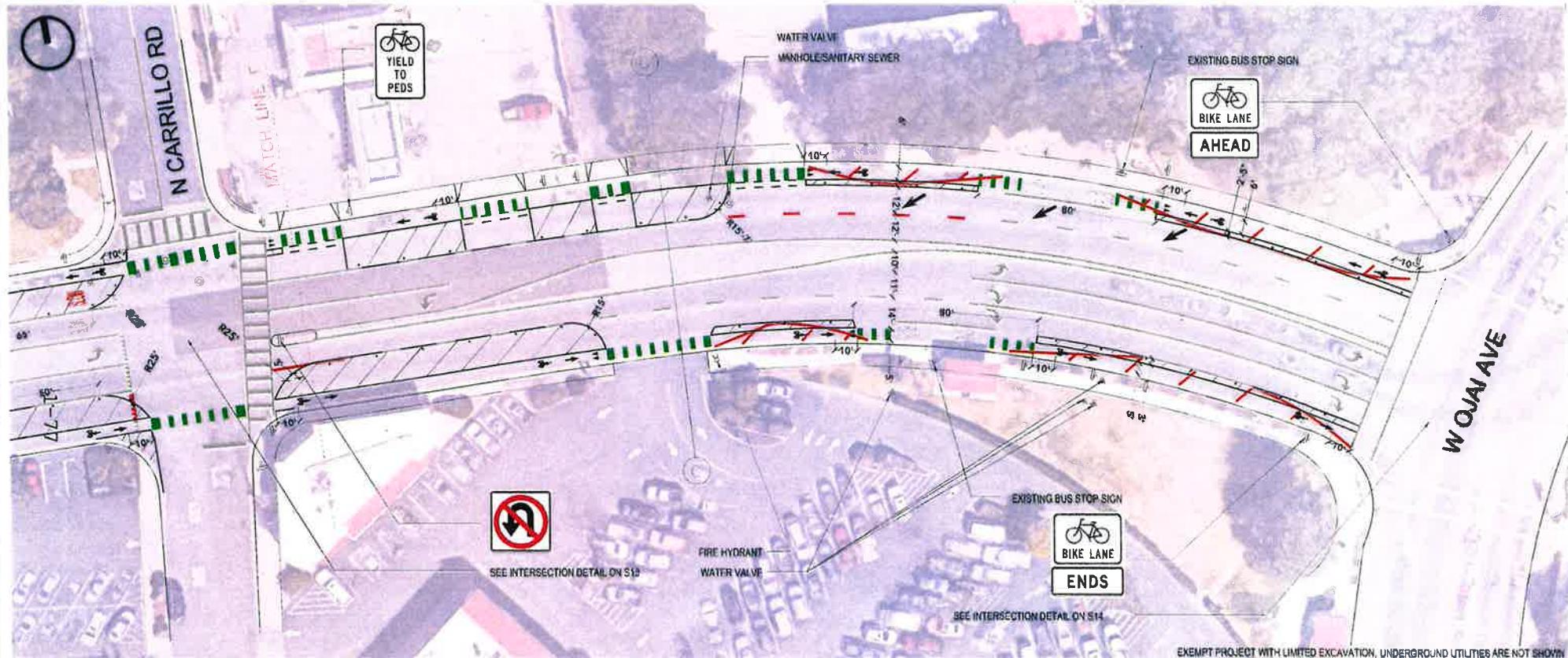
S7 MARICOPA HWY
STRIPING PLAN
DRAWING TITLE
REVIEWS SUBMISSIONS
#104/07/2020 #106/07/2020 #3/07/17/2020
#2/10/2020 #2/11/2020 #4/06/21/2019 (FINAL)
#4/06/21/2019 (FINAL)
1:20 SCALE

NOTES

- 1 EXISTING STRIPING AND PAVEMENT MARKINGS ARE SHOWN IN GRAY. STRIPING AND PAVEMENT MARKINGS SHOWN IN RED, WHICH CONFLICT WITH THE PROPOSED BIKE LANE, ARE TO BE REMOVED DURING DEMONSTRATION.
- 2 PROPOSED BIKE LANE MARKINGS SHOW IN BLACK, AND CONFLICT ZONE MARKINGS IN GREEN
- 3 STRIPING: WHITE SHERWIN WILLIAMS HD LINE PAINT
- 4 DELINEATORS: ZICLA CYCLE LANE SEPARATOR, PLANTERS: ZICLA ZEBRA PLANTER (SEE S15-S17)
5. SIGNAGE: MUTCD SIGNS TO BE AFFIXED TO EXISTING POLES WHERE INDICATED, PROJECT BANNERS TO BE AFFIXED TO EXISTING LIGHT POLES WHERE INDICATED (SEE PDF PAGE 19 FOR MORE DETAILS)

STREET SECTION 'F'





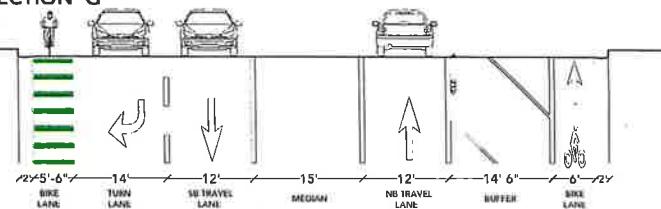
DEMONSTRATION PROJECT SITE PLAN

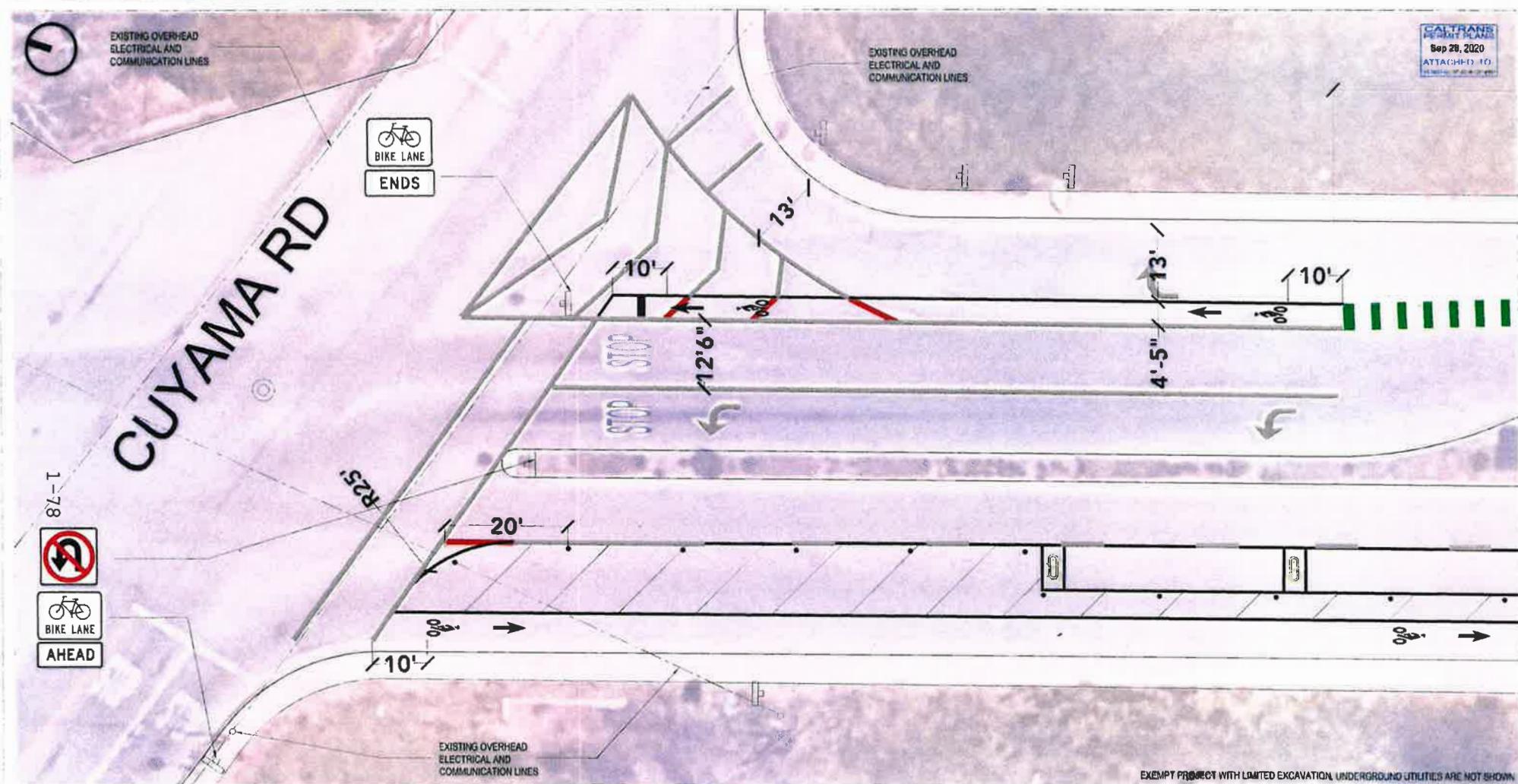
DEMONSTRATION PROJECT
CALTRANS DISTRICT 7
PERMIT # 720-AOP-2411

NOTES

- EXISTING STRIPING AND PAVEMENT MARKINGS ARE SHOWN IN GRAY. STRIPING AND PAVEMENT MARKINGS SHOWN IN RED, WHICH CONFLICT WITH THE PROPOSED BIKE LANE, ARE TO BE REMOVED DURING DEMONSTRATION
- PROPOSED BIKE LANE MARKINGS SHOW IN BLACK, AND CONFLICT ZONE MARKINGS IN GREEN
- STRIPPING: WHITE SHERWIN WILLIAMS HOLLOWLINE PAINT
- DELINERATORS: ZICLA CYCLE LANE SEPARATOR, PLANTERS: ZICLA ZEBRA PLANTER (SEE S15-S17)
- SIGNAGE: MUTCD SIGNS TO BE AFFIXED TO EXISTING POLES WHERE INDICATED, PROJECT BANNERS TO BE AFFIXED TO EXISTING LIGHT POLES WHERE INDICATED (SEE PDF PAGE 19 FOR MORE DETAILS)

STREET SECTION 'G'





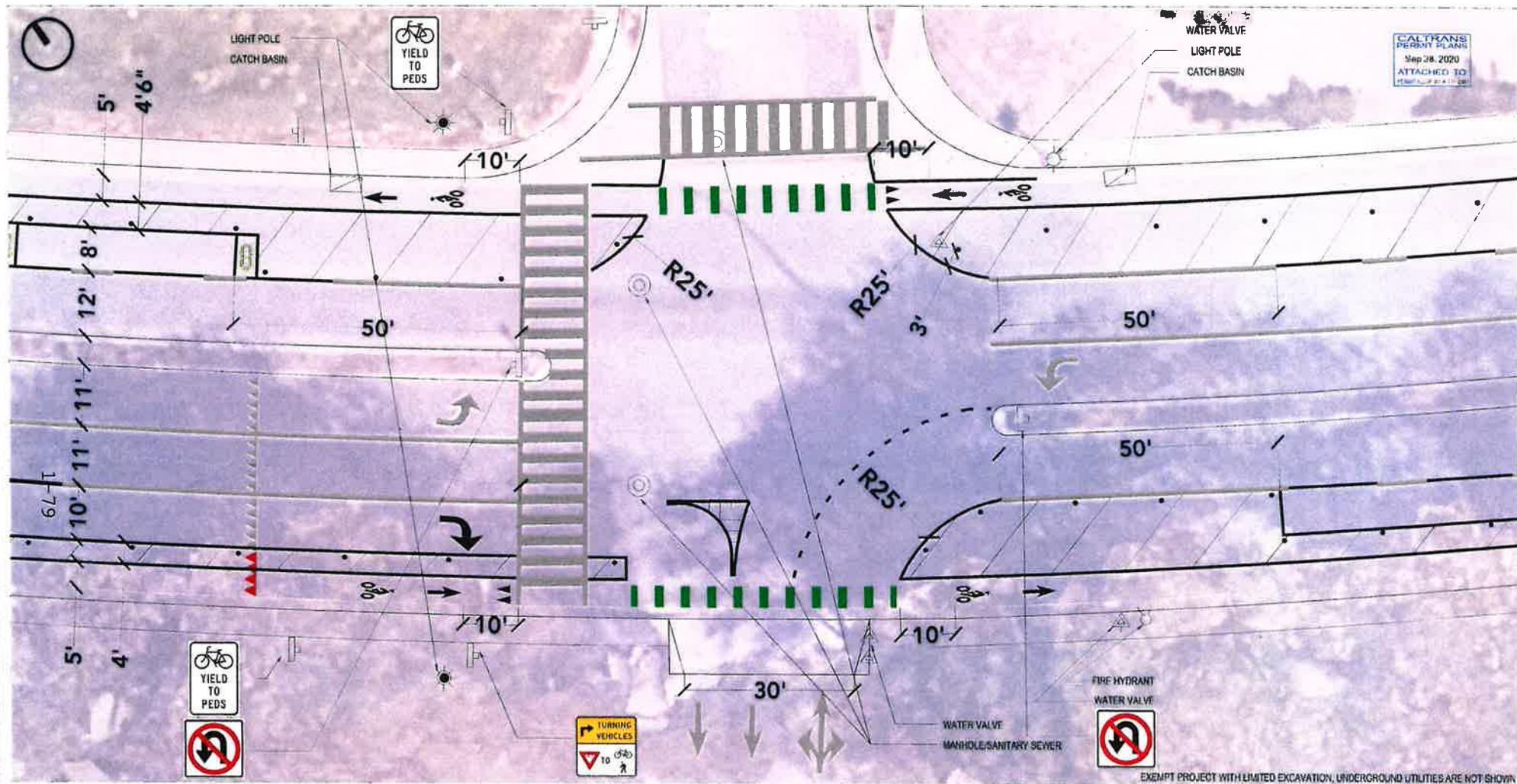
CUYAMA RD INTERSECTION DETAIL

NOTES

1. EXISTING STRIPPING AND PAVEMENT MARKINGS ARE SHOWN IN GRAY. STRIPPING AND PAVEMENT MARKINGS SHOWN IN RED, WHICH CONFLICT WITH THE PROPOSED BIKE LANE, ARE TO BE REMOVED DURING DEMONSTRATION
2. PROPOSED BIKE LANE MARKINGS SHOW IN BLACK, AND CONFLICT ZONE MARKINGS IN GREEN
3. STRIPPING: WHITE SHERWIN WILLIAMS HOTLINE PAINT
4. DELINEATORS: ZICLA CYCLE LANE SEPARATOR, PLANTERS: ZICLA ZEBRA PLANTER (SEE PDF PAGES S15-S17)
5. SIGNAGE: MUTCD SIGNS TO BE AFFIXED TO EXISTING POLES WHERE INDICATED, PROJECT BANNERS TO BE AFFIXED TO EXISTING LIGHT POLES WHERE INDICATED (SEE PDF PAGE 19 FOR MORE DETAILS)

UTILITY LEGEND

- LIGHT POLE
- MANHOLE / SANITARY SEWER
- ◊ WATER VALVE
- CATCH BASIN
- ✗ FIRE HYDRANT
- EXISTING SIGN POLES



CHURCH RD INTERSECTION DETAIL

Attachment H
STREET PLANS
Page 11 of 15

S10 MARICOPA HWY STRIPING PLAN

REVIEWS SUBMISSIONS
#1 04/09/2020 #1 06/23/2020 #3 07/31/2020
#2 04/09/2020 #2 07/10/2020 #4 08/21/2020 (FINAL)
#5 09/11/2020 (NOV CENT)
SCALE: 1/8" = 1' - 0"

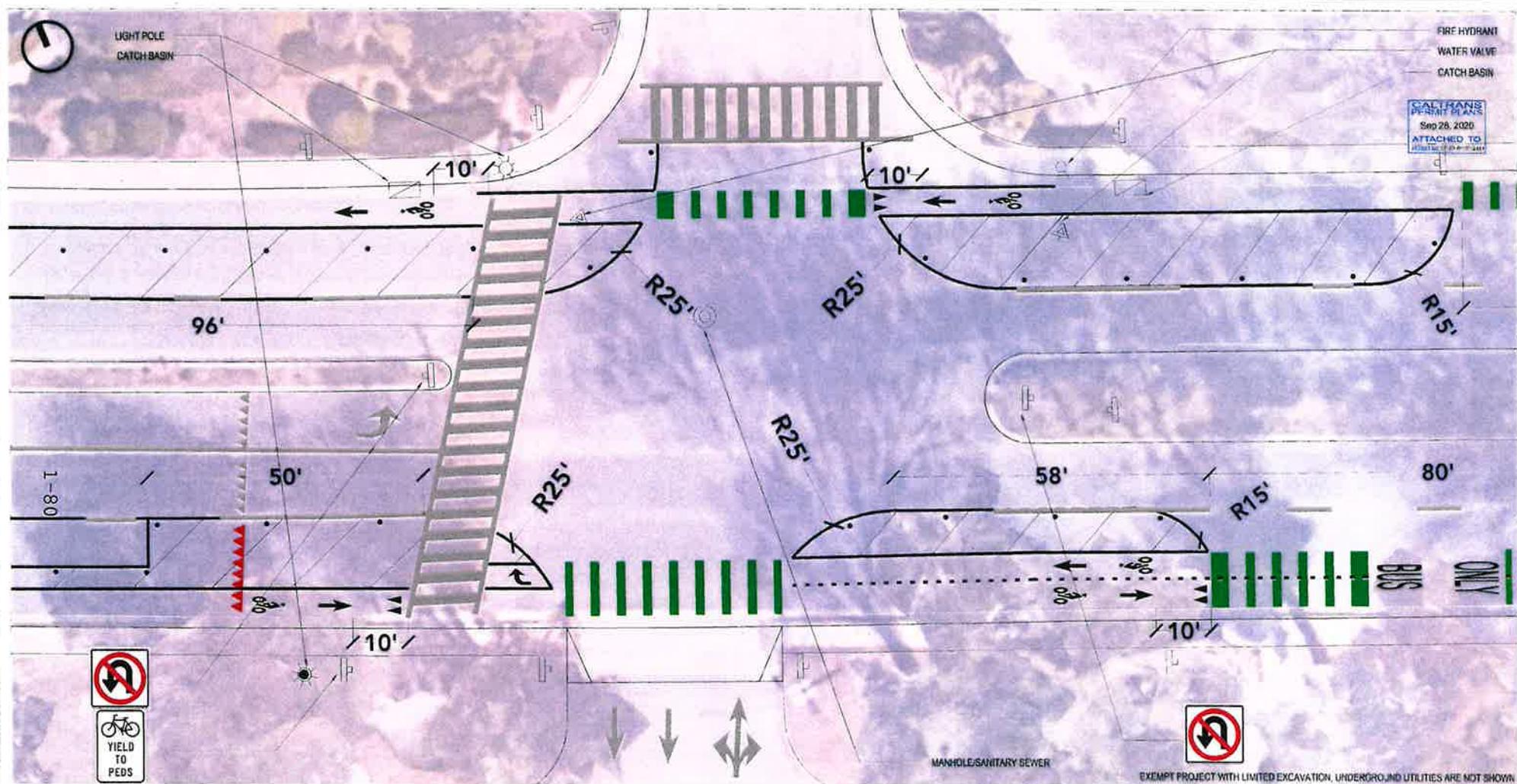
NOTES

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- STRIPING: WHITE SHERWIN WILLIAMS HOTLINE PAINT
- DELINERATORS: ZICLA CYCLE LANE SEPARATOR, PLANTERS: ZICLA ZEBRA PLANTER (SEE S15-S17)
- SIGNAGE: MUTCD SIGNS TO BE AFFIXED TO EXISTING POLES WHERE INDICATED, PROJECT BANNERS TO BE AFFIXED TO EXISTING LIGHT POLES WHERE INDICATED (SEE PDF PAGE 19 FOR MORE DETAILS)

UTILITY LEGEND

- LIGHT POLE
- MANHOLE SANITARY SEWER
- △ WATER VALVE
- CATCH BASIN
- FIRE HYDRANT
- EXISTING SIGN POLES

DEMONSTRATION PROJECT
CALTRANS DISTRICT 7
PERMIT # 720-AOP-2411



PIRIE RD INTERSECTION DETAIL

Attachment
Page 12 of 15
STREETPLANS
STREET PLANS INC
TRANSFORMATION DESIGN - RESEARCH
1010 SHERMAN DRIVE, SUITE 4
SOUTH MIAMI, FL 33143
(305) 976-9420 | STREETPLANS.ORG

S11 MARICOPA HWY
STRIPING PLAN
DRAWING TITLE
REVIEWS SUBMISSIONS
4/1/2020 4/1/2020
4/2/2020 4/2/2020 (FINAL)
4/2/2020 4/2/2020 (ROW CERT)
1/8" = 1' - 0" SCALE

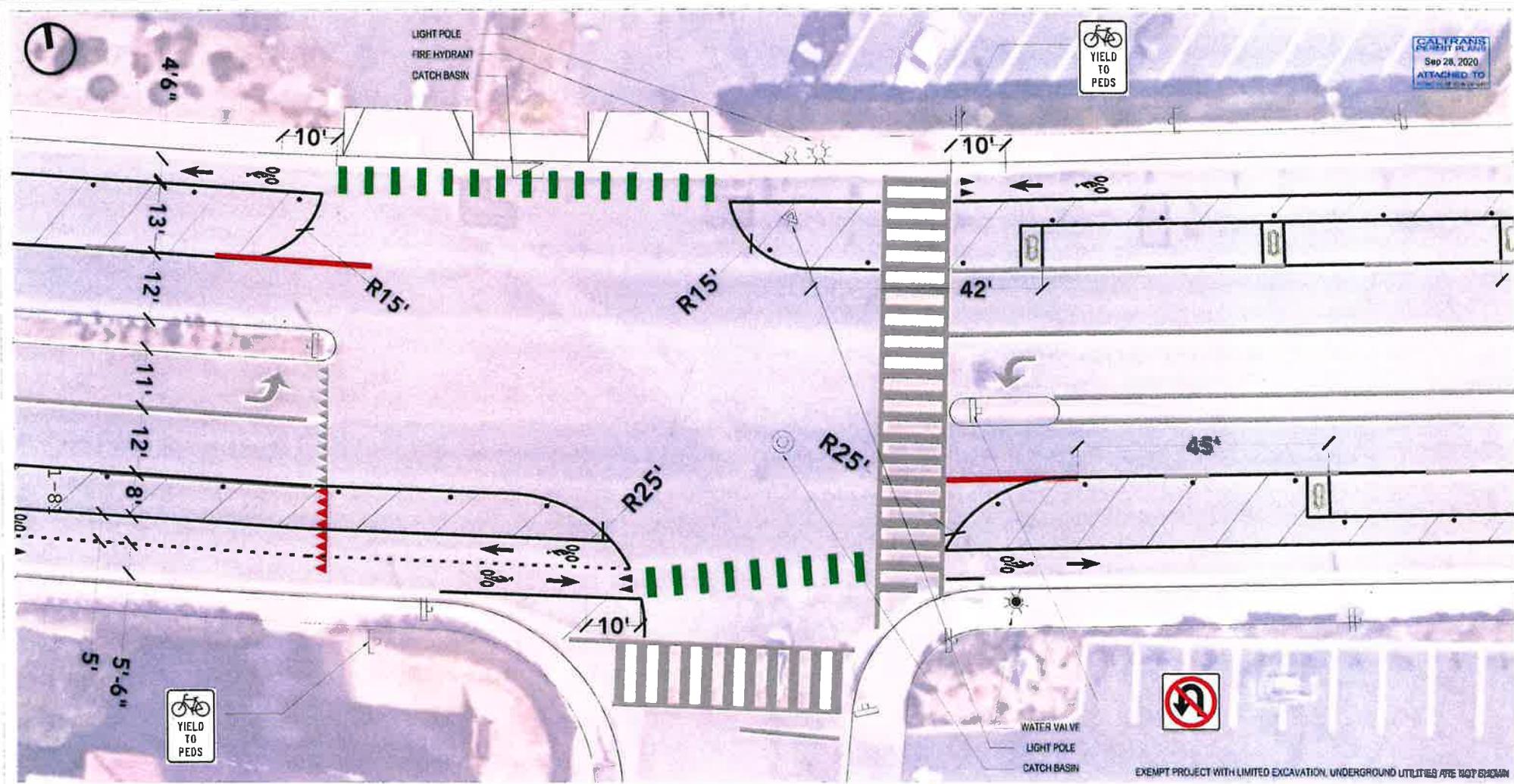
NOTES

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- 2 PROPOSED BIKE LANE MARKINGS SHOW IN BLACK, AND CONFLICT ZONE MARKINGS IN GREEN
- 3 STRIPING: WHITE SHERWIN WILLIAMS HOTLINE PAINT
- 4 DELINEATORS: ZICLA CYCLE LANE SEPARATOR, PLANTERS: ZICLA ZEBRA PLANTER (SEE S15-S17)
- 5 SIGNAGE: MUTCD SIGNS TO BE AFFIXED TO EXISTING POLES WHERE INDICATED, PROJECT BANNERS TO BE AFFIXED TO EXISTING LIGHT POLES WHERE INDICATED (SEE PDF PAGE 19 FOR MORE DETAILS)

UTILITY LEGEND

- Light pole
- Manhole / sanitary sewer
- Water valve
- Catch basin
- Fire hydrant
- Existing sign poles

DEMONSTRATION PROJECT
CALTRANS DISTRICT 7
PERMIT # 720-AOP-2411



VALLERIO AVE INTERSECTION DETAIL

S12 MARICOPA HWY
DRAWING TITLE

REVIEWS
7/10/17/2020
#764019265
1/8" = 1' - 0"
SCALE

SUBMISSIONS
#3/31/2020
#1 US6332024
#2 07/10/2020
#3 US21212020 (FINAL)
#4 US21212020 (L11)

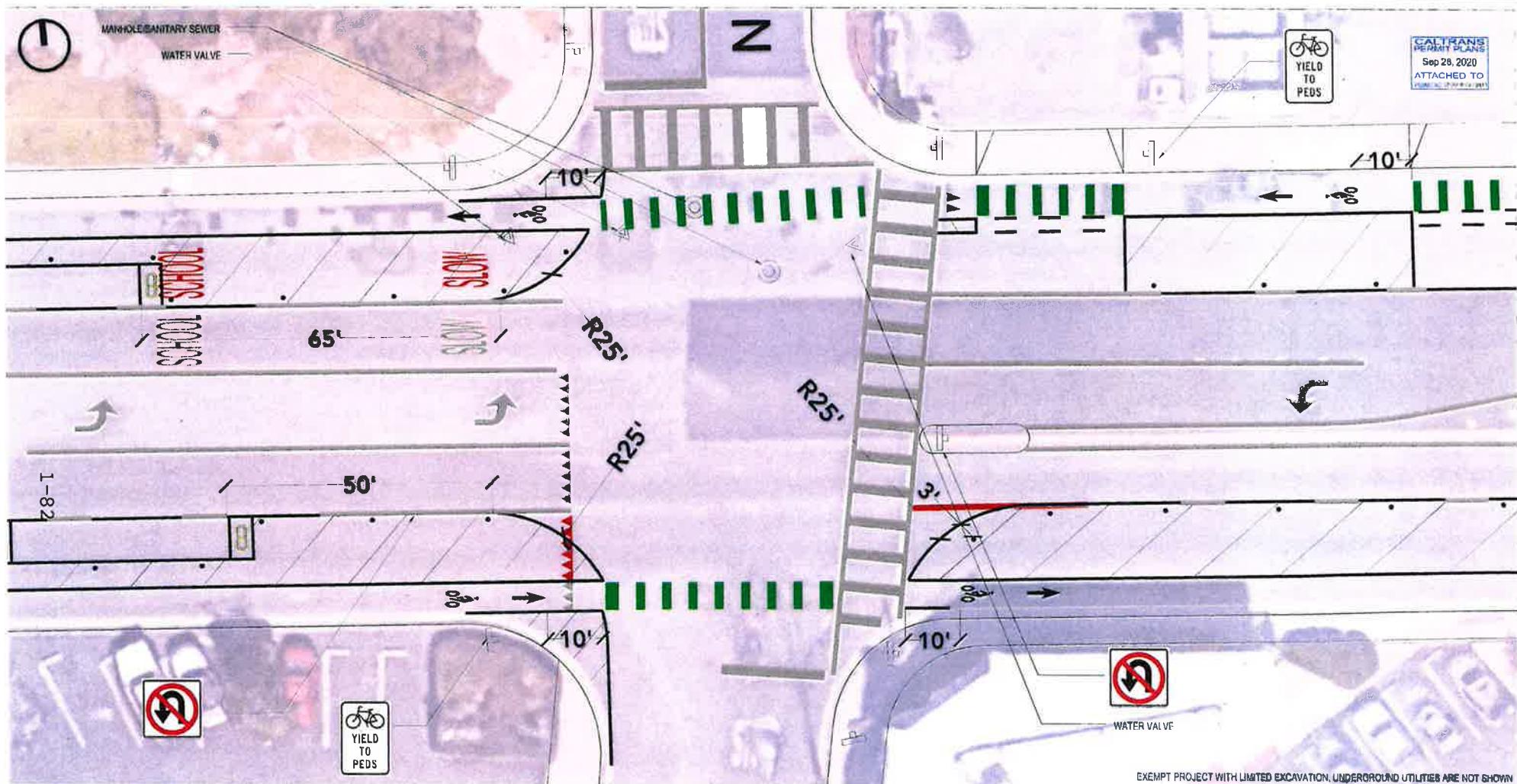
NOTES

- 1 EXISTING STRIPING AND PAVEMENT MARKINGS ARE SHOWN IN GRAY. STRIPING AND PAVEMENT MARKINGS SHOWN IN RED, WHICH CONFLICT WITH THE PROPOSED BIKE LANE, ARE TO BE REMOVED DURING DEMONSTRATION
- 2 PROPOSED BIKE LANE MARKINGS SHOWN IN BLACK, AND CONFLICT ZONE MARKINGS IN GREEN
- 3 STRIPING: WHITE SHERWIN WILLIAMS HOTLINE PAINT
- 4 DELINEATORS ZICLA CYCLE LANE SEPARATOR, PLANTERS ZICLA ZEBRA PLANTER (SEE PDF PAGES S15-S17)
- 5 SIGNAGE: MUTCD SIGNS TO BE AFFIXED TO EXISTING POLES WHERE INDICATED. PROJECT BANNERS TO BE AFFIXED TO EXISTING LIGHT POLES WHERE INDICATED (SEE PDF PAGE 18 FOR MORE DETAILS)

UTILITY LEGEND

-  LIGHT POLE
-  MANHOLE / SANITARY SEWER
-  WATER VALVE
-  CATCH BASIN
-  FIRE HYDRANT
-  EXISTING SIGN POLES

**DEMONSTRATION PROJECT
CALTRANS DISTRICT 7
PERMIT # 720-AOP-2411**



N CARRILLO RD INTERSECTION DETAIL

Attachment 15
STREETPLANS
 Page 14 of 15

S13 MARICOPA HWY
STRIPING PLAN

REVIEWS	SUBMISSIONS	
#1000072020	#1 06/23/2020	#3 07/31/2020
#2 04/20/2020	#2 07/16/2020	#4 08/21/2020 (FINAL)
		#5 08/11/2020 (BROW GERN)
1/8" = 1' - 0"		
SCALE		

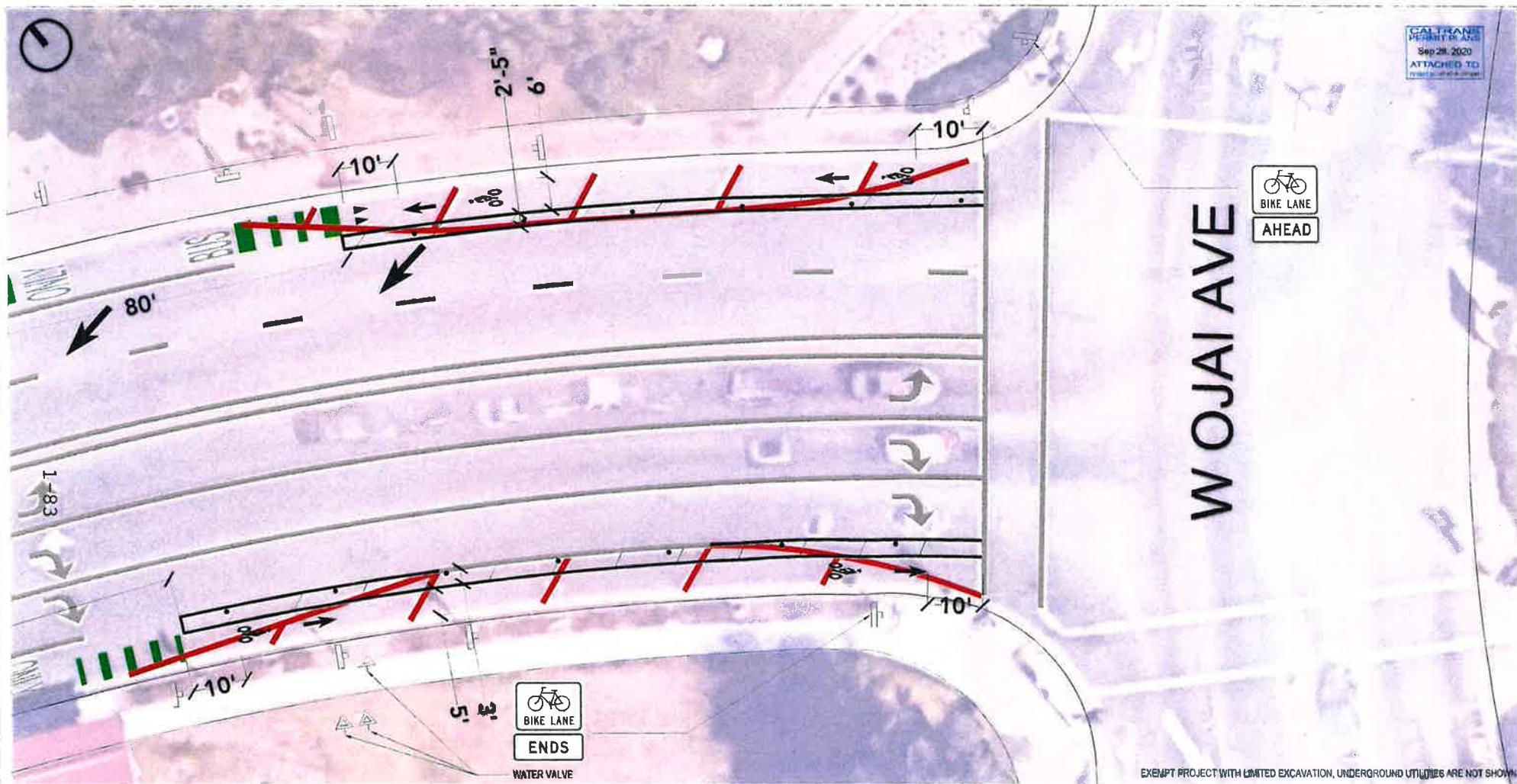
NOTES

- 1 EXISTING STRIPING AND PAVEMENT MARKINGS ARE SHOWN IN GRAY. STRIPING AND PAVEMENT MARKINGS SHOWN IN RED, WHICH CONFLICT WITH THE PROPOSED BIKE LANE, ARE TO BE REMOVED DURING DEMONSTRATION
- 2 PROPOSED BIKE LANE MARKINGS SHOW IN BLACK, AND CONFLICT ZONE MARKINGS IN GREEN
- 3 STRIPING: WHITE SHERWIN WILLIAMS HOTLINE PAINT
- 4 DELINEATORS: ZICLA CYCLE LANE SEPARATOR, PLANTERS: ZICLA ZEBRA PLANTER (SEE PDF PAGES S15-S17)
- 5 SIGNAGE: MUTCD SIGNS TO BE AFFIXED TO EXISTING POLES WHERE INDICATED, PROJECT BANNERS TO BE AFFIXED TO EXISTING LIGHT POLES WHERE INDICATED (SEE PDF PAGE 19 FOR MORE DETAILS)

UTILITY LEGEND

- LIGHT POLE
- MANHOLE / SANITARY SEWER
- WATER VALVE
- CATCH BASIN
- FIRE HYDRANT
- EXISTING SIGN POLES

DEMONSTRATION PROJECT
CALTRANS DISTRICT 7
PERMIT # 720-AOP-2411



W OJAI AVE INTERSECTION DETAIL

Attachment
Page 15 of 15
STREET PLANS
STREET PLANNING
PLANNING, DESIGN, RESEARCH
1000 KIRKET DRIVE, SUITE 4
SAN JOSE, CA 95131-1244
360-976-0420 | STREETPLANS.ORG

S14 MARICOPA HWY
STRIPING PLAN
DRAWING TITLE
REVIEWS SUBMISSIONS
#9-04082020 #11-02-2020 #07/21/2020
#9-04082020 #11-02-2020 #11-02-2020 (FINAL)
#09-04-2020 #11-02-2020 #09-04-2020 (WATER VALVE)
10' x 1' 0" SCALE

NOTES

1. EXISTING STRIPING AND PAVEMENT MARKINGS ARE SHOWN IN GRAY. STRIPING AND PAVEMENT MARKINGS SHOWN IN RED, WHICH CONFLICT WITH THE PROPOSED BIKE LANE, ARE TO BE REMOVED DURING DEMONSTRATION
2. PROPOSED BIKE LANE MARKINGS SHOW IN BLACK, AND CONFLICT ZONE MARKINGS IN GREEN
3. STRIPING: WHITE SHERWIN WILLIAMS HOTLINE PAINT
4. DELINEATORS: ZICLA CYCLE LANE SEPARATOR, PLANTERS: ZICLA ZEBRA PLANTER (SEE PDF PAGES S15-S17)
5. SIGNAGE: MUTCD SIGNS TO BE AFFIXED TO EXISTING POLES WHERE INDICATED, PROJECT BANNERS TO BE AFFIXED TO EXISTING LIGHT POLES WHERE INDICATED (SEE PDF PAGE 19 FOR MORE DETAILS)

UTILITY LEGEND

- ★ LIGHT POLE
- MANHOLE / SANITARY SEWER
- ◆ WATER VALVE
- ▷ CATCH BASIN
- FIRE HYDRANT
- EXISTING SIGN POLES

DEMONSTRATION PROJECT
CALTRANS DISTRICT 7
PERMIT # 720-AOP-2411

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Attachment I

Traffic – Volume and Speed Data Summary

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Traffic - Volume and Speed Data Summary

Week:	Vehicle Speed (avg, 50th%, mph)				Vehicle Volume (ADT, vehicles/day)			
	(Church/Meadows)		(Pirie/Church)		(Church/Meadows)		(Pirie/Church)	
	NB	SB	NB	SB	NB	SB	NB	SB
Pre-Demo:								
10/20-24, 2020	37.6	36.9	38.0	33.8	3,491	3,494	3,911	3,771
10/25-31, 2020	37.7	37.0	38.0	33.9	3,416	3,430	4,109	4,089
11/1-7, 2020	37.8	36.4	38.1	33.8	3,731	3,691	4,206	4,077
11/8-12, 2020	42.4	37.7	38.8	34.9	2,946	2,955	4,010	3,866
Average:	38.9	37	38.2	34	3,396	3,393	4,059	3,951
Average (all):				37.1				3,700
Post-Demo:								
1/8-14, 2021	36.5	32.9	34.7	33.6	3,112	3,161	3,471	3,371
1/15-21, 2021	37.4	32.5	35.1	34.1	3,301	3,385	3,654	3,591
1/22-28, 2021	37.1	34.2	35.0	33.9	3,234	3,377	3,626	3,424
Average:	37.0	33	34.9	34	3,216	3,308	3,584	3,462
Average (all):				34.8				3,392
					5% decrease	3% decrease	12% decrease	12% decrease

Average
8% decrease

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